

9th Avenue SE Streetscape Master Plan

2018.09.05





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Brian Brer

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Executive Summary

Background

9th Avenue SE, between 8th Street and 20th Street in Inglewood is currently an auto oriented corridor where the existing conditions have created an unwelcoming pedestrian environment. The current streetscape conditions require pedestrians and cyclists to navigate cluttered and narrow sidewalks, loud and fast moving vehicle traffic and vehicle oriented intersections. Many areas of the public realm are in poor and deteriorating condition. With discontinuous Main Street streetscape improvements, and a wide variety of adjacent private property setback conditions 9th Avenues character varies from block to block.

Currently the physical makeup of the street prioritizes vehicle movement above other mobility choices, with the majority of the ROW being allocated to roadway operations. This is at odds with 9th Avenues Main Street designation. However, in spite of 9th Avenues challenging pedestrian environment the community of Inglewood has grown to become one of Calgary's unique, diverse, eclectic, and COOL communities. Inglewood and the Main Street the community is built around is an irreplaceable heritage asset and a living story of Calgary's history. There is latent potential for 9th Avenue's transformation to support and grow the economic vitality, creative culture and increase the local population density. 9th Avenue needs to be a vibrant Main Street destination that supports the unique community.

As part of the Main Streets initiative the Streetscape Master Plan provides:

- A comprehensive summary of known conditions, issues, and constraints.
- Analysis, and recommendations on potential improvement opportunities.
- Design criteria and concept development.
- Analysis of alternative options.
- A preferred concept design for the 9th Avenue SE corridor.
- A summary class 5 cost estimate.
- Future operational and maintenance requirements.
- Implementation and phasing strategy.

The recommended design concept takes consideration of the many other capital projects in the community such as the Future Green Line, BRT, 9th Avenue and 12th Street bridges, and active Main Street mixed use developments.

This plan sets in place a strong vision for public realm enhancements and development that unify communities, reinforce a sense of place, and enhance the pedestrian experience by creating a pedestrian focused public realm.

Process

The 9th Avenue Streetscape Master Plan Study included a significant number of stakeholders who were engaged over a 18 month period. The work was a collaborative effort, involving community members, local businesses owners, and the greater public; as well as City staff representing most major departments. A series of group Walkshops and one-on-one sessions were conducted. From these a collective vision and key design principles were developed.

Vision

With a remarkable array of businesses and people that share a passion for the well-being of the community the Master Plan for 9th Avenue re-imagines the street as a place that will foster and grow its creative culture and economic vitality. The design provides a framework allowing the street to evolve over time to reflect the eclectic and messy community character of Inglewood. The master plan recommends 9th Avenue as a people place focusing on transit and pedestrian priority. The design provides for a variety of mobility options to facilitate efficient transit movement while recommending the slowing of vehicles to create a walkable, pedestrian focused environment. 9th Avenue is to be an integrated aspect of the bike network providing connections to the adjacent 8th Avenue bike route, and to the regional pathway network. The plan reallocates space within the ROW to prioritize pedestrians and provide the community with public gathering areas. By providing wider sidewalks, flexible parking areas that also provide patio opportunities and a design that is uniquely Inglewood, 9th Avenue will become a destination that attracts people and the investment that follows.

The recommended Master Plan includes:

- A 4 lane roadway: 2 east bound lanes, 1 westbound lane, 1 permanent parking lane
- A transit priority lane and off-peak parking on the south side and a permanent parking lane on the north.
- Wider sidewalk areas for pedestrians which features new concrete paving, reclaimed pavers, pedestrian scale lighting, a family of furnishings and material to suit the community character, and respond to the context of the river and the railway lines.
- Increased area for unique community and public gathering along the street.
- Integrated public art, and proposed concept design for the Blackfoot underpass.

Realization of the plan requires a phased redevelopment approach that considers the current conditions and usage, complexity of the proposed changes including interface with surface and buried utilities, alignment and timing with other adjacent capital projects and appropriate sequencing for multi-modal movements.

1. Introduction

Background and Purpose of the Plan

Welcome to Inglewood

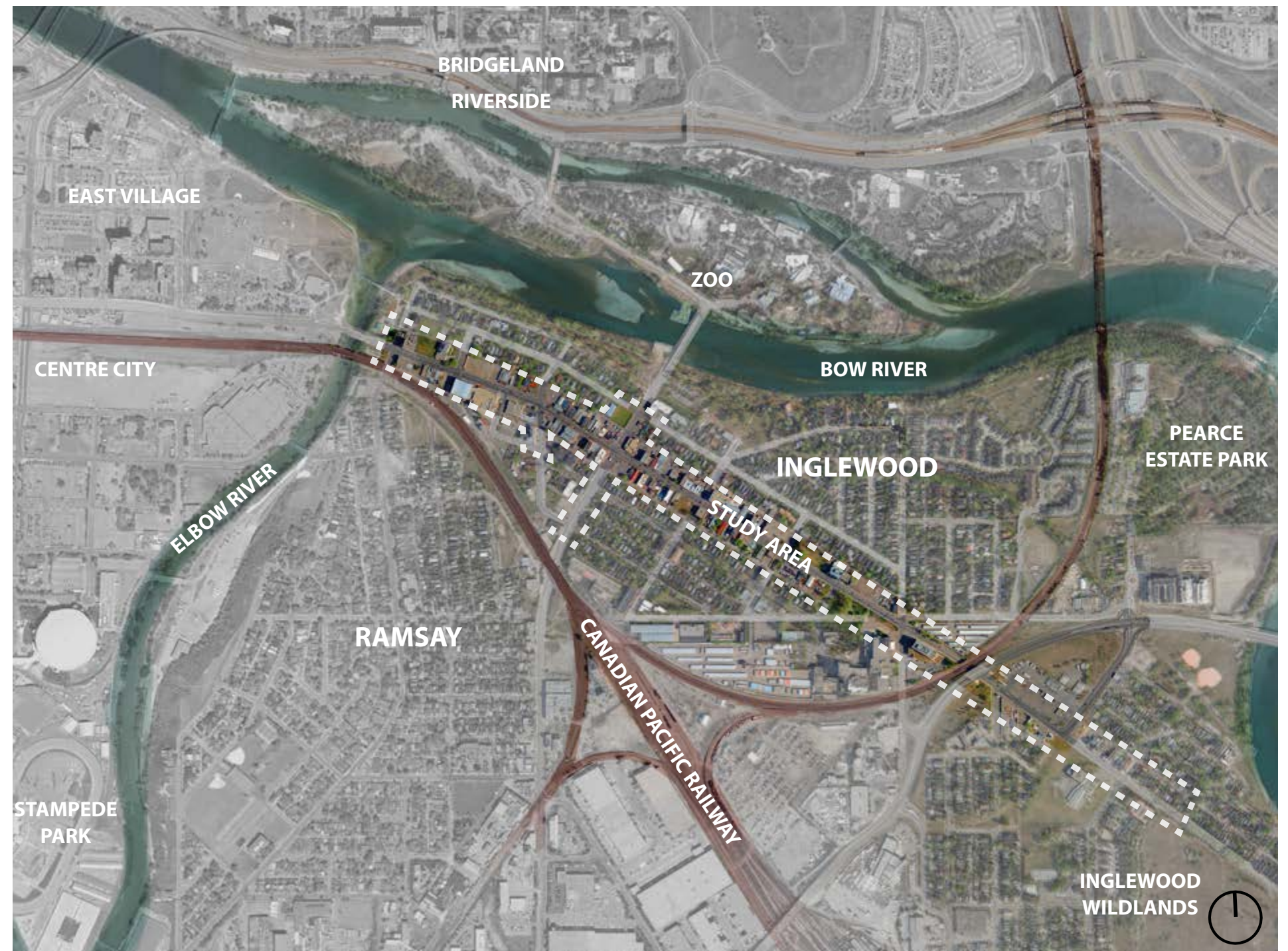
The community of Inglewood is unique, diverse, eclectic, and COOL! Not surprisingly Inglewood was named Canada's Greatest Neighbourhood in 2013 by the Canadian Institute of Planners. Inglewood is a great place to visit, and a great place to live. Inglewood offers a range of housing opportunities and building types, multi-modal transportation options that support community facilities, and a diversity of local and unique businesses. 9th Avenue has a vibrant bar and theatre scene, a variety of restaurant experiences, and is a cultural destination and home to many artists. The community is uniquely bounded by the Elbow River to the West, the Bow River and open space to the North, the Canadian Pacific Railway to the South, and Inglewood Bird Sanctuary and Inglewood Wildlands and the Bow River to the East. It is close to everything, but a world apart. Creating a great Main Street environment on 9th Avenue is a vital part of meeting community aspirations of becoming the quintessential urban village and creating an Inglewood that is compact, connected, and pedestrian oriented.

Inglewood's 9 Avenue SE Main Street, originally named Atlantic Avenue, stretches from the Elbow River to 17 Avenue SE. It is located south of the Calgary Zoo and the East Village, and is a home for the communities of Inglewood and Ramsay, and a destination for the greater Calgary area. The scope for the 9th Avenue Streetscape Master Plan runs from 8th Street SE to 20th Street SE, and also includes a segment from 12th Street from 8th Avenue to the north to 11th Avenue at the south. Additionally side streets and community connections are considered as well as 2 Laneways (alleys) located between 9th Avenue and 8th Avenue from 13th Street SE to 15th Street SE.

With the growth and evolution of the community there is a need to re-imagine 9th Avenue as a great Main Street destination, that can support and foster the aspirations of this unique and eclectic community.

Purpose

- Set the framework of the public realm for creating a great Main Street destination
- Builds upon previous work and engagement associated with the Main Streets program
- Provides a snapshot of key issues
- Coordinates other capital initiatives in the community
- Principles and recommendations of the final concept design, once approved by the Transportation Leadership Team, will be developed for construction pending funding



STUDY AREA CONTEXT MAP

Related Plans and Policies and Parallel Projects

The 9th Avenue Streetscape Master Plan builds upon other City of Calgary plans, guidelines, and policies that direct the development, growth and character of the community and relate to transportation, planning, and urban design in the community. The Streetscape Master plan began with a general inventory and analysis of the wider community area and a review of the existing documents and plans guiding development in the community. A number of City of Calgary documents informed the project objectives and street typologies, including:

- Municipal Development Plan;
- Calgary Transportation Plan;
- Complete Streets Policy and Guide;
- Engage Policy and Engage Framework;
- Public Art Policy;
- Public Art Strategic Direction - 2011;
- Inglewood Area Redevelopment Plan, draft 2018;
- Downtown Underpass Urban Design Guidelines;
- Centre City Illumination Guidelines;
- Cycling Strategy;
- Pedestrian Strategy;
- A Parking Policy Framework for Calgary;
- 2020 Sustainability Direction;
- Inglewood Design Initiative (IDI);
- Inglewood Ninth Avenue SE Design Guidelines;
- Activity Centre and Main Street Mixed-Use Districts;
- Main Streets: What We've Heard;
- Green Line Plans;
- Adjacent Capital Projects;
- Proposed Developments in the Corridor.

Parallel Projects

There are a number of capital projects in the community such as the 9th Avenue bridge replacement, the new Green Line alignment and 12th Street station, and the new 17th Avenue S.E. BRT projects. These capital projects will change the character of the community and will be coordinated with the proposed Streetscape design works.

- New 9th Avenue SE Bridge
- New 12th Street SE Bridge
- Pathway Upgrades
- Zoo Island + Zoo Road: protected for 1:100 flood
- Green Line
- Mills park Improvements
- Jack Long Park Improvements
- 17th Avenue BRT
- Green Line Elbow river crossing
- 25th Ave SE LRT Grade separation study
- Rivers District Master plan
- Area Redevelopment Plans
- Sanitary Trunk Upgrades
- Harvey Passage Recovery Project
- Bend in the Bow Redevelopment Plan

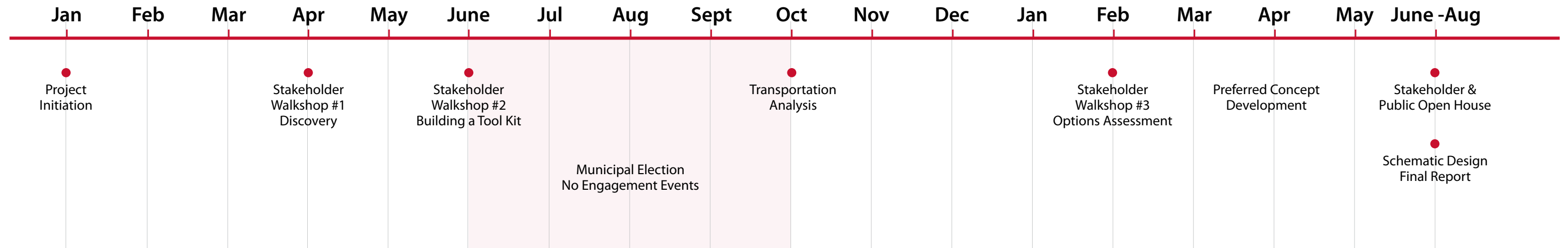
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Master Planning Process



2017

2018



About the Process

It was critical to understand the views and interests of residents, businesses, pedestrians, commuters and shoppers in relation to the existing 9th Avenue streetscape, and its future. The engagement process featured a series of walkshops where a targeted group of stakeholders were engaged to develop guiding principles and vision for the street as well as evaluate and inform potential design options. This process was supported by public online engagement and sounding boards and multiple public open houses. The process aimed to achieve inclusive and meaningful stakeholder engagement by creating an environment where all stakeholders voices were heard and those voices influenced the next phase of concept development. With many other concurrent project engagement activities, such as the Green Line, BRT, ARP updates, the engagement process needed to be unique to limit potential stakeholder engagement fatigue.

The primary goals of the stakeholder engagement were as follows:

- Provide innovative and meaningful engagement activities.
- Develop a sense of community ownership for the project.
- Provide an inclusive and fun process.
- Minimize barriers to stakeholder input and potential stakeholder fatigue.



INPUT FROM WALKSHOP #1

Stakeholder Engagement Events

Walkshop 1 & Online Engagement

Discovery

The first phase of engagement focused on identifying the existing issues with 9th Avenue and discovering what makes the street unique eclectic and funky. A targeted group of stakeholders, armed with Polaroid cameras and sticky notes formed walking groups and took to the streets to discuss opportunities, issues, constraints and community character. This was followed by street kiosks, street decals and an Online engagement process.

Walkshop 2 & Online Engagement

Building a Tool Kit

The second phase of engagement focused on gathering feedback on design elements to be considered for the street. Through a targeted stakeholder engagement session feedback was collected on the following input points: the street elements of 9th Avenue, side streets, crosswalks and intersections, laneways and park interfaces, and public art. The event participants took the presentation boards to the street to discuss the design elements in-situ to develop a strong understanding of the specifics of 9th Avenue. This was followed up with a street kiosk and online engagement process.



PHOTOS FROM WALKSHOP #1 & #2



Walkshop 3 & Online Engagement Overview

Refining the Options

The third phase of engagement was a presentation of three potential options and direction for the Streetscape master plan. With a targeted group of stakeholders three concepts were presented and discussed, with the concepts varying in lane configuration and allocation of pedestrian space. In groups, character elements were discussed in depth to develop an understanding of a preferred outcome for the community. This event was followed by an open house and an online engagement process.

Open House Overview

Preferred Concept

An open house was held presenting the preferred concept to the public. This event allowed for a final point of input prior to the submission of the concept report to TLT, and a presentation of the entirety of the process to the community.



PHOTOS FROM WALKSHOP #3 AND OPEN HOUSE

Discovery Phase Walkshop #1

What Did we Hear

APRIL 2017

- Sidewalks are Narrow
- Pedestrian Crossings are Unsafe
- The Public Realm is Auto-Oriented
- Open Space is Isolated
- There are Limited Gathering Areas
- Vehicle Movement is Loud and Fast
- Many Pedestrian Obstructions on Side Walks
- Surface Parking Creates a Discontinuous Building Street Wall
- Limited Space for Street Side Patios
- Limited Diversity of Open Space
- Limited Pedestrian Connections to Adjacent and Local Destinations
- Minimal Showcase of Local Artists and Art Throughout the Street
- Lack of 'Inglewood' Character

“Wider sidewalks, less of a focus on pushing traffic through the community.”

“Way too narrow. Should have bump outs to make road easier to cross”

“Focus on major intersections to beautify/ highlight connection to the river + Pedestrian routes....”

“Old Buildings - New Ideas. Keep historic sites & revitalize. Keep character. Keep the Brewery! ”

Community Engagement

- 20 Street Decals
- Online Questionnaire
- Community Kiosk
- 453 Visitors
- 294 Feedback Responses



Photos : Stakeholder Walkshop April 2017

What Did we Do

- Developed a community and stakeholder view of the issues with existing conditions.
- Developed a community-led vision for what 9th Avenue can be.

Tool Kit Walkshop #2

What Did we Hear

Street Character

Wider sidewalks with opportunities for patios, integrated street furniture and enhanced planting

Crosswalks + Street Corners

Wider, artful, signalized crosswalks with corner curb extensions for transit and gathering areas and rain gardens

Side Streets

Opportunities for gathering, wayfinding and enhanced planting

Laneway + Park Connections

Provide integrated lane and park interface with seating and community gathering opportunities

Public Art

Integration of a wide variety and scale of art interventions



“Anything that activates corners/intersections and helps slow the pace of traffic is good”

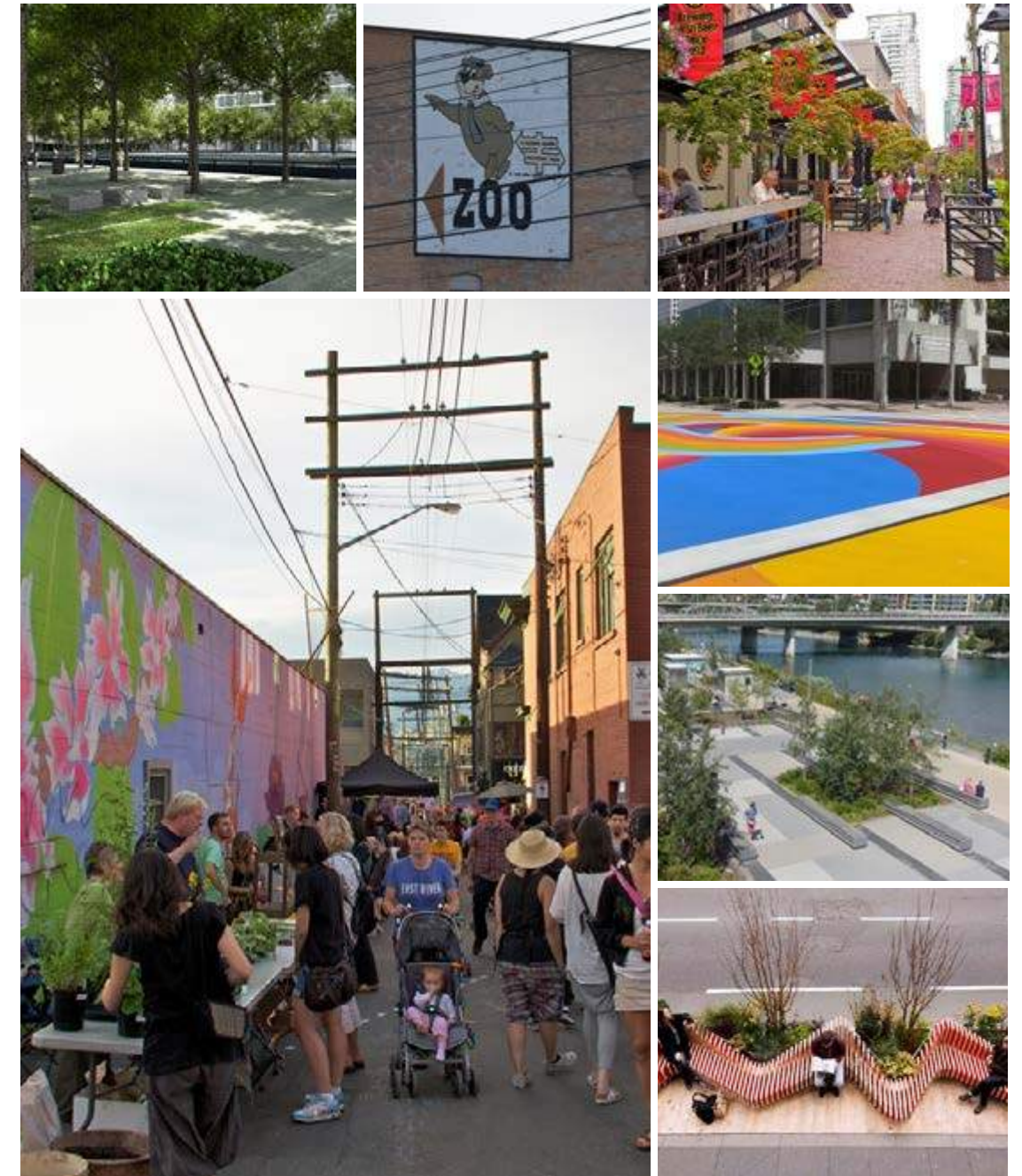
“This is Inglewood...art is a must. More opportunity to showcase community and local artists”

“Side street opportunities for gathering...music art planting opportunities”

“More natural feel is encouraged...more trees, more planting, better access to parks is needed”

What Did we Do

- Development of a unique to Inglewood Streetscape toolkit.
- Development of a strong vision for what the community needs and identified strategies for the variety of potential places along 9th Avenue.



Walkshop # 2: Street DNA + Character Areas

Concept Options Walkshop #3

What was Presented

The third walkshop presented three potential options for 9th Avenue that varied based on laneing configuration, allocation of pedestrian space, provision for public gathering area and opportunities for planting and street furnishing. These options were presented and discussed in groups to establish what the community liked most, what was missing, what could be removed and which laneing and pedestrian space allocations best aligned with the community led vision for 9th Avenue.

To achieve the pedestrian focused vision a percentage of right-of-way area was provided to illustrate space allocation and prioritization along the street. Trade-offs and opportunities were discussed to gain insight on community and stakeholder preferences. The percentage incorporates an average of corner bulb areas, flexible parking area for patios and sidewalk space to make up the pedestrian realm. The cross sections differ in where and if these elements can be provided.

4 Lanes Option 1

Lane Narrowing

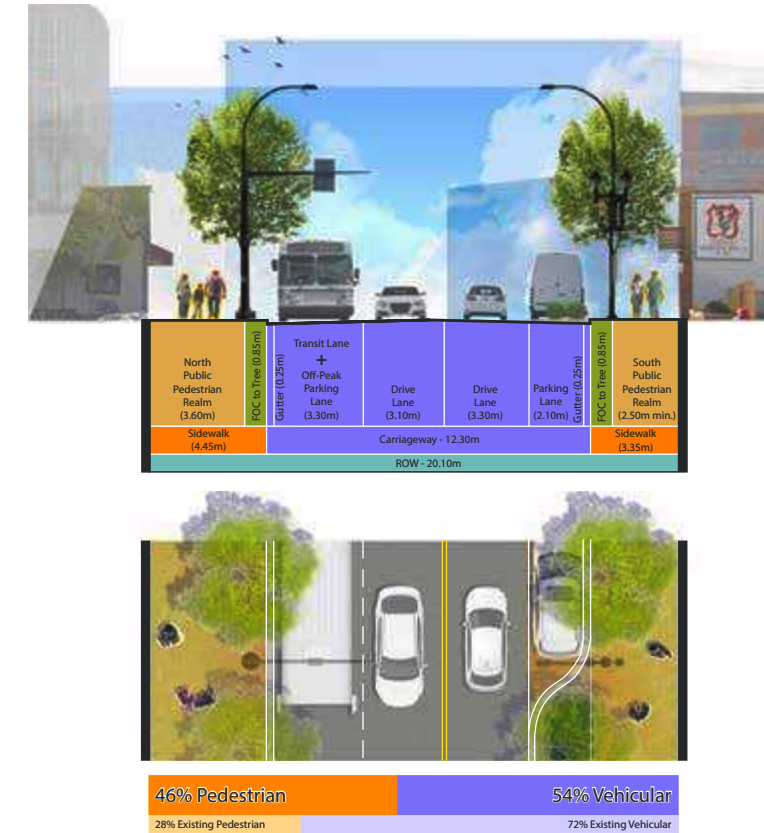


Design Elements

- Two Travel Lanes
- Two Transit Lanes + Off-Peak Parking
- Moderately Wider Sidewalks
- Extensions at Corners on Side Streets Only
- Signalized Pedestrian Crossings + Wider Artful Crosswalks
- Pedestrian Oriented Lighting + Power for Seasonal Light Displays
- Reclaimed Brick Paver Accents – Community Character
- Dedicated Transit Lanes to Accommodate Bike Traffic
- New Street Trees with Appropriate Soil Volumes
- Bollards to Define Pedestrian Edge
- Public Art Opportunities
- Rain Garden + Storm Water Management Opportunities
- Integrated Street Furniture

4 Lanes Option 2

3 Drive Lanes
1 Parking Lane



Design Elements

- Two Travel Lanes
- One Transit Lane + Off-Peak Parking
- One Lane Parking
- Wider Sidewalks
- Extensions at Corners + Transit Stops Along South Side
- Signalized Pedestrian Crossings + Wider Artful Crosswalks
- Room for Sidewalk Patios + Informal Seating on North Side
- Pop-Up Patio Opportunities on South Side
- Pedestrian Oriented Lighting + Power for Seasonal Light Displays
- Reclaimed Brick Paver Accents – Community Character
- Dedicated Transit Lane to Accommodate Bike Traffic
- New Street Trees with Appropriate Soil Volumes
- Bollards to Define Pedestrian Edge
- Public Art Opportunities
- Rain Garden + Storm Water Management Opportunities
- Integrated Street Furniture + Gathering Areas

4 Lanes Option 3

3 Drive Lanes

1 Parking Lane

Lane Reversal with Gantries



Design Elements

- Two Travel Lanes
- Peak Transit Lanes + Off-Peak Parking
- One Lane Parking
- Lane Reversal Gantries
- Wider Sidewalks
- Extensions at Corners + Transit Stops Along South Side
- Signalized Pedestrian Crossings + Wider Artful Crosswalks
- Room for Sidewalk Patios + Informal Seating on North Side
- Pop-Up Patio Opportunities on South Side
- Pedestrian Oriented Lighting + Power for Seasonal Light Displays
- Reclaimed Brick Paver Accents – Community Character
- Dedicated Transit Lane to Accommodate Bike Traffic
- New Street Trees with Appropriate Soil Volumes
- Bollards to Define Pedestrian Edge
- Public Art Opportunities
- Rain Garden + Storm Water Management Opportunities
- Integrated Street Furniture + Gathering Areas

Concept Options Walkshop #3

FEBRUARY 2018

What Did we Hear

Concept Design

- Endorsement for design elements.
- Endorsement for option 2.
- Ensure functionality of 9th Ave for transit operations.
- Address potential traffic diversions resulting from lane reduction.
- Ensure gathering areas will provide opportunities for the community.

Public Art

- Endorsement for the concept.
- Ensure maintenance and operational concerns are addressed.
- Ensure concept works throughout all hours of the day and night.
- Include local artists in the process.
- Public art should be integrated throughout Streetscape design.

Street Composition

- Endorsement for option 2.
- Preference towards pedestrian focused environment.
- Provide design elements to slow vehicles and increase pedestrian safety
- Desire to push design further towards 2 travel lane configuration.

Side Streets

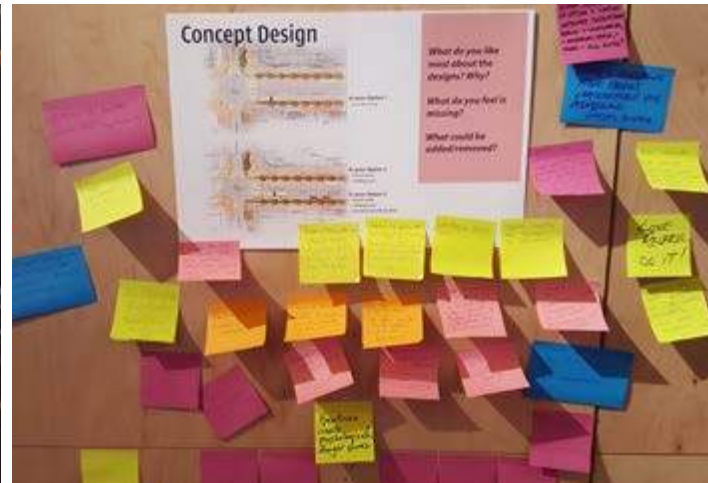
- Endorsement for the concept
- Ensure areas are provided for patios to support local businesses.
- Ensure generous tree canopy.
- Ensure there is appropriate facilities for cycling.

Crosswalks + Street Corners

- Endorsement for the concept.
- Support for the inclusion of corner extensions for added amenity.
- Ensure crosswalks and design elements reflect Inglewood character.
- Ensure there is adequate facilities for bikes.

Laneway + Park Interface

- Endorsement for the concept.
- Ensure design addresses parking and use issues for surrounding land uses.
- Ensure seating and gathering areas are functional and safe.



Photos : Stakeholder Walkshop February 2018

Concept Options Walkshop #3: What Did we Do

- Provided a preferred concept that integrated input on the streetscape DNA elements.
- Developed a preferred option that prioritized pedestrians by allocating more area to pedestrians in the right-of-way.

“Encourage bicycles + pedestrian access through 9th Ave. to downtown to connect Ramsay+ Blackfoot underpass”

“Yes, build it!!!”

“Like these - they are just so ‘Inglewood’! Shout out to railway/ industrial, and the river. Very eclectic.”

Community Engagement

- Open House
- Online Questionnaire
- 697 Visitors
- 235 Feedback Responses

Preferred Concept Open House

JUNE 2018

The Big Reveal

A final open house was held with the public and stakeholders that present the master plan process, input from previous engagement and the preferred concept. A end to end plan was presented along with a enlarged concept design and concepts for laneways, public art, street composition, side streets, crosswalks and street corners and park interfaces. Sticky notes provided a final point of input for the public and stakeholders, with any comments being integrated in to the final concept report.



Photos : Public Open House June 2018

Preferred Concept Open House: What Did we Do

- Ensured that the public and community had meaningful input and involvement in the process.
- Ensured the preferred option is endorsed by the community, public and stakeholders.

Vision, Principles & Objectives

Context

9th Avenue is the community heart of Inglewood and is the fabric that ties together the area's patchwork of unique and electric character. Drawing on elements from the river to the north and the railway to the south, 9th Avenue is a Main Street destination and a place that fosters the growth of the creative community ecosystem. Above everything 9th Avenue is a street for people, a street that creates opportunities for the community and a street that acts as a front door for creative local businesses. The street is a canvas for artists and can change with the seasons providing vibrancy year round. Its a place where people come to live, work and play, and a place that can continue to evolve over time with the surrounding community.

Design Principles

- Efficient Transit Movement
- Pedestrian Focused
- Slow Vehicles and Walkable Street
- Foster Creative Culture
- Create Community and Public Gathering Areas
- Make a Better Destination and Build Vibrancy
- Improve Connections and Enhance Wayfinding
- Promote Environmental Resiliency

THE VISION IS TO CREATE A GREAT MAIN STREET DESTINATION THAT CAN SUPPORT AND FOSTER THE ASPIRATIONS OF THIS UNIQUE AND ECLECTIC COMMUNITY.



Main Streets Program Approach

Main Streets Program Overview

The Main Streets Program was established to foster long-term transformation and sustainable population growth in key established communities throughout the city. From 2014 through 2016, The City undertook extensive public engagements, performed economic and market demand studies, and completed area planning reviews to identify 24 Main Streets throughout Calgary.

Vision

Calgary's Main Streets are places where citizens come together. They are places where we want to go, we enjoy, and we coalesce as a community. The transformation of Main Streets will occur jointly through public and private investment, and will empower citizens to travel less and live more by providing the things we need right now in our own communities.

Principles

Main Streets are resilient, adaptable, and attractive public spaces that:

- Celebrate the **character** of the community;
- Encourage **diversity** of local businesses, buildings and residents, and;
- Create a vibrant **destination**

City-wide Engagement Findings

Through the extensive public engagement process in 2014-2015, City planners and staff heard what main street users and citizens would like to see for the future of their main streets. Common themes for future improvements across all of Calgary's Main Streets included:

- Vibrant public realm;
- Creating a destination;
- Public and park space improvements;
- Unique character;
- Balance of transportation options;
- Housing diversity, and;
- A variety of retail and small businesses

Turning Vision into Action

Creating A Unique Street Ecosystem

From the input gathered throughout the process and the community led vision for 9th Avenue, strategies were developed to deliver a concept for the street that could achieve this vision. The strategies below will be applied to realize the design principles and transform 9th Avenue into a Main Street destination.

Efficient Transit Movement

- Provide transit priority lanes.
- Improve signal timing to facilitate movement.
- Provide on street and bus bulb transit stops.

Pedestrians Focused

- Provide wider sidewalks.
- Improve pedestrian crossings.
- Improve pedestrian scale lighting.
- Provide places to mingle.

Promote Environmental Resiliency

- Provide street trees and extensive plantings.
- Optimizing soil volume for tree and planting health.
- Provide rain gardens and permeable paving.

Create Community and Public Gathering Areas

- Provide corner and mid block bulbs.
- Provide pedestrian furniture integrated into the streetscape.
- Improve pedestrian scale lighting.
- Provide park interface improvements.
- Provide flexible and adaptable parking areas.

Make a Better Destination and Build Vibrancy

- Capitalize on context, river and railway.
- Create a sense of entry to the community.
- Provide a streetscape design palette and elements that reflect the character of the community.

Improve Connection and Enhance Wayfinding

- Improve visibility and accessibility to external destinations.
- Connect Inglewood and Ramsay.
- Provide connections for multi-modal mobility.

Slow Vehicles and Walkable Street

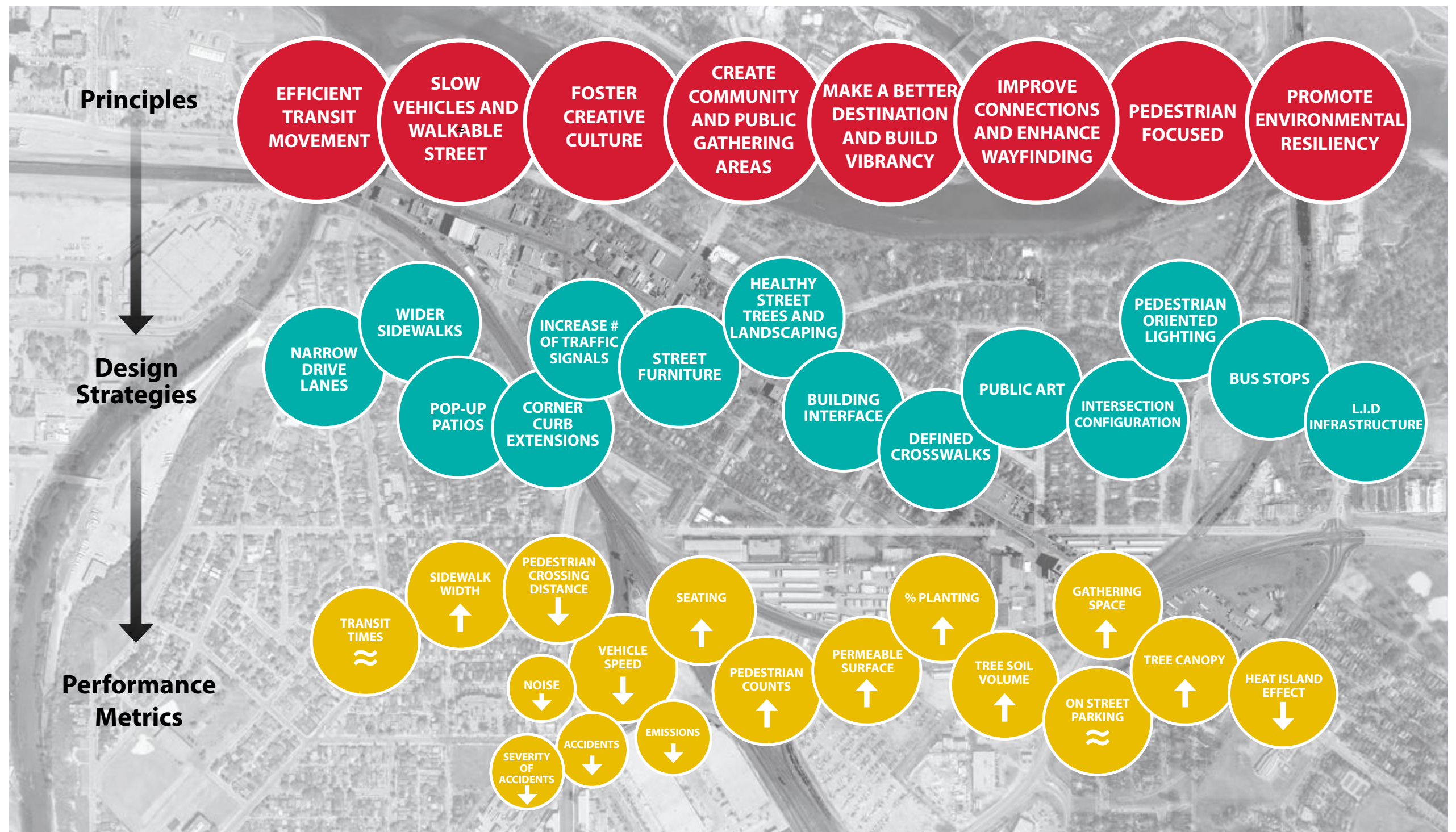
- Provide signalized intersections at 9th Street, 11th Street, 13th Street, 14th Street and 16th Street.
- Improve signalized intersection at 8th Street, 12th Street, 17th ave and 19th Street.
- Provide pedestrian activated crossings at 10th Street, 14a Street and 20th Street.
- Improve pedestrian and transit prioritized signal timing.
- Provide optimized drive lane width.
- Limit posted speed to 30km/h. Consideration for reducing posted vehicular speeds will be evaluated further in subsequent phases.

Foster Creative Culture

- Provide opportunities for local artists and integrated public art.
- Provide venues for community and public gathering.
- Provide flexible design for street to evolve with community..

Objectives

Creating a great Main Street requires the correct suite of design elements that can address the vision and design principles.



2. Site Context: Understanding 9th Avenue

Historical Context

Inglewood is Calgary's oldest civilian settlement. It was established after Fort Calgary was built in 1875, and home to the city's original Main Street. Since the beginning, Inglewood has been a vibrant self-contained community. Its identity was forged by the physical location between the Elbow River, the Bow River, and the Canadian Pacific Railway. Originally known as East Calgary or Brewery Flats the community developed around its main street (originally called Atlantic Avenue) and included a fire hall, Police station, two schools, three churches, and the Bank of Commerce that has been in operation since 1911. There are still 400 historic buildings remaining in Inglewood, many with historic designation. The Area Redevelopment Plan (ARP) has classified Inglewood as a Special Heritage District.

It is important to remember that for centuries before any European settlement in Inglewood First Nations gathered here. It has a long history as a gathering place, a place for trade, and a place for habitation. This location originated as a transportation hub, where the rivers came together, where historic trails converged, and where the railway first arrived in Calgary. The City of Calgary has grown up around this gathering space.

Inglewood has had its fair share of hard times. The Great Depression and the post-war migration of families to the suburbs took its toll on the community, but through it all it was the investment of the residents in their own community through countless volunteer hours which shaped what Inglewood is today.

Inglewoodians have gumption! Without the tenacity of the community of Inglewood, there would be no community of Inglewood. Inglewood has integrity, and will fight with determination and passion to uphold what they hold dear to their hearts to be an inclusive community that is compassionate, diverse and connected. With over 100 years of investment from the community, Inglewood has successfully stood up to development that is counter to its ideals. The community even fought back when The City proposed plans for a freeway directly through Inglewood in the 60's and the community won. With a long track record of investment from residents and volunteers, supported by the community association and the Business Improvement Area (BIA) have been integral in shaping their community through the development of the Inglewood Design Brief in the 80's, support of the ARP in the 90's and more recently the development of the Inglewood Design Initiative and updated ARP.



9th AVENUE BRIDGE



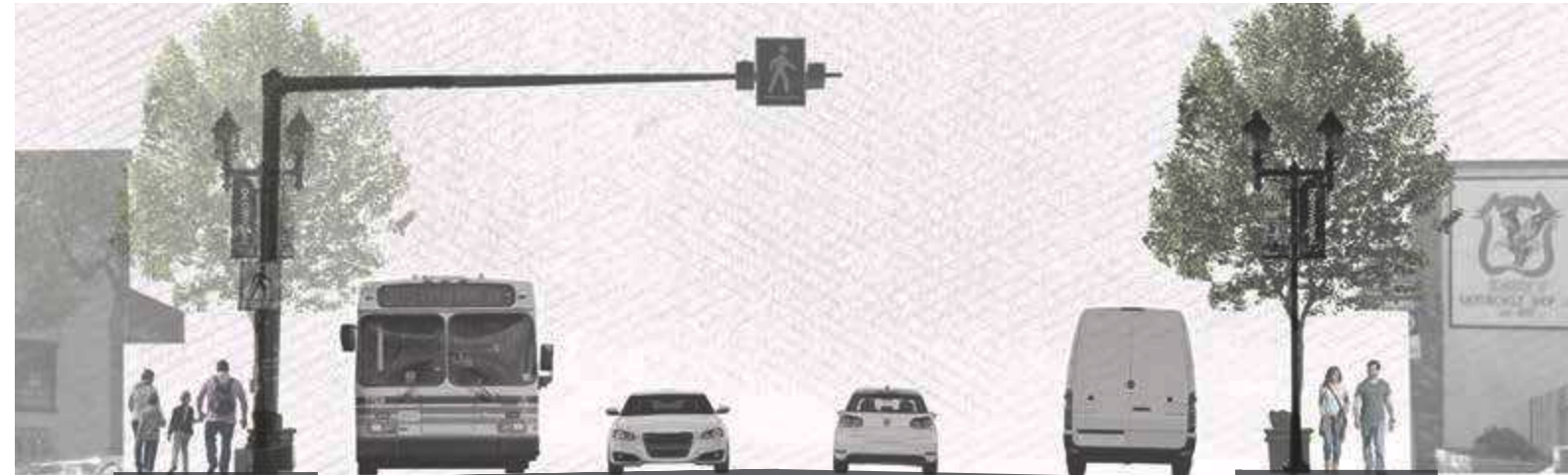
9th AVENUE STREETCAR

Transit, Vehicular Circulation, Access + Parking

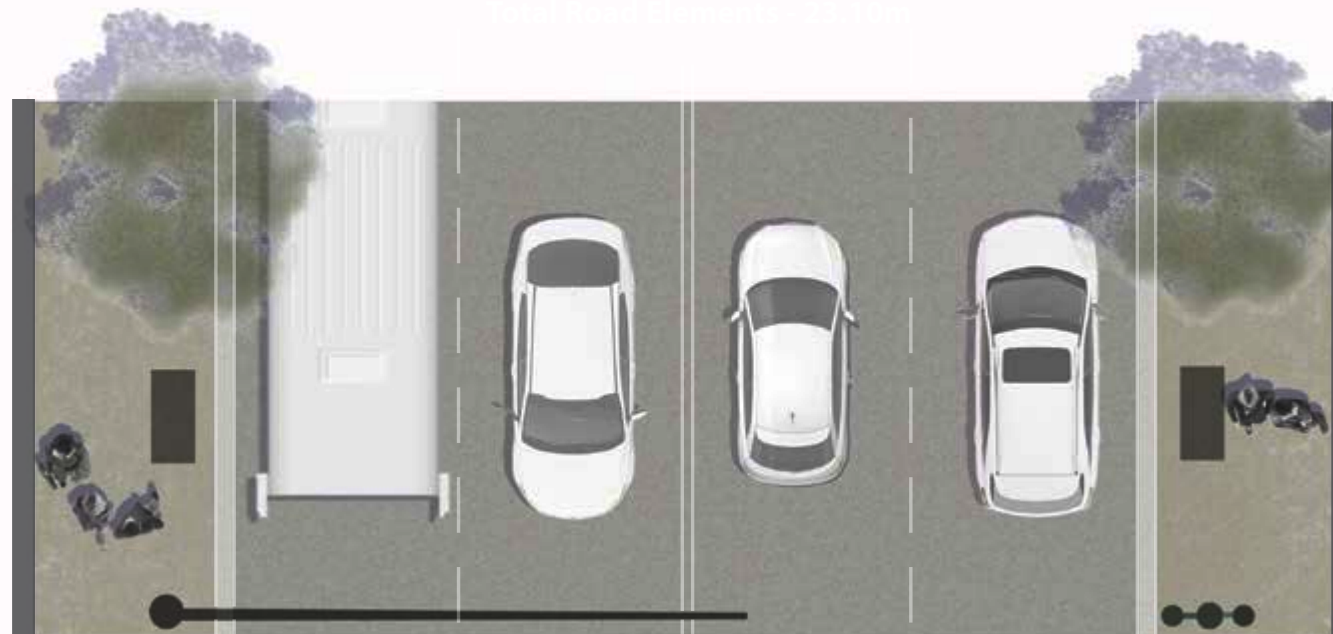
9th Avenue is classified as a Main Street and Neighbourhood Boulevard, which places policy priorities on pedestrians, creating a vibrant public realm, and slowing vehicle speed. Currently 9th Avenue is an auto oriented street with many issues impacting the pedestrian environment. With a wide carriageway and a consistent right-of-way of 20.10m sidewalks are generally narrow and clustered with loud, fast moving traffic creating an unwelcoming street to pedestrians and cyclists. There have been recent streetscape improvements along 9th Avenue but the pedestrian environment is still largely constrained.

Currently 9th Avenue only provides a narrow sidewalk area, with limited to no opportunity for corner bulbs and patio areas. As a result, a large amount of space within the right-of-way is allocated to vehicles. Citizens and engagement participants were vocal in sharing that 9th Avenue was an unwelcoming, and at times unsafe, pedestrian environment.

Existing plan and cross section of R.O.W



North Public Pedestrian Realm (1.98m)	FOC to Tree (0.85m)	Gutter (0.25m)	Transit Lane and Off-Peak Parking (3.50m)	Travel Lane (3.50m)	Travel Lane (3.50m)	Transit Lane and Off-Peak Parking (3.50m)	Gutter (0.25m)	FOC to Tree (0.85m)	South Public Pedestrian Realm (1.93m)
Sidewalk (2.83m)		Carriageway - 14.50m					Sidewalk (2.78m)		
ROW - 20.10m									



28% Existing Pedestrian

72% Existing Vehicular



Barrier Between Pedestrian Realm And Open Park Space



Insufficient Width For Street Furnishings and Pedestrian Circulation



Prohibited Pedestrian Crossings



Winter Maintenance Conditions Create Barriers For Pedestrians



Vehicle Oriented Public Realm



Narrow, Cluttered Sidewalks

Transit, Vehicular Circulation, Access + Parking

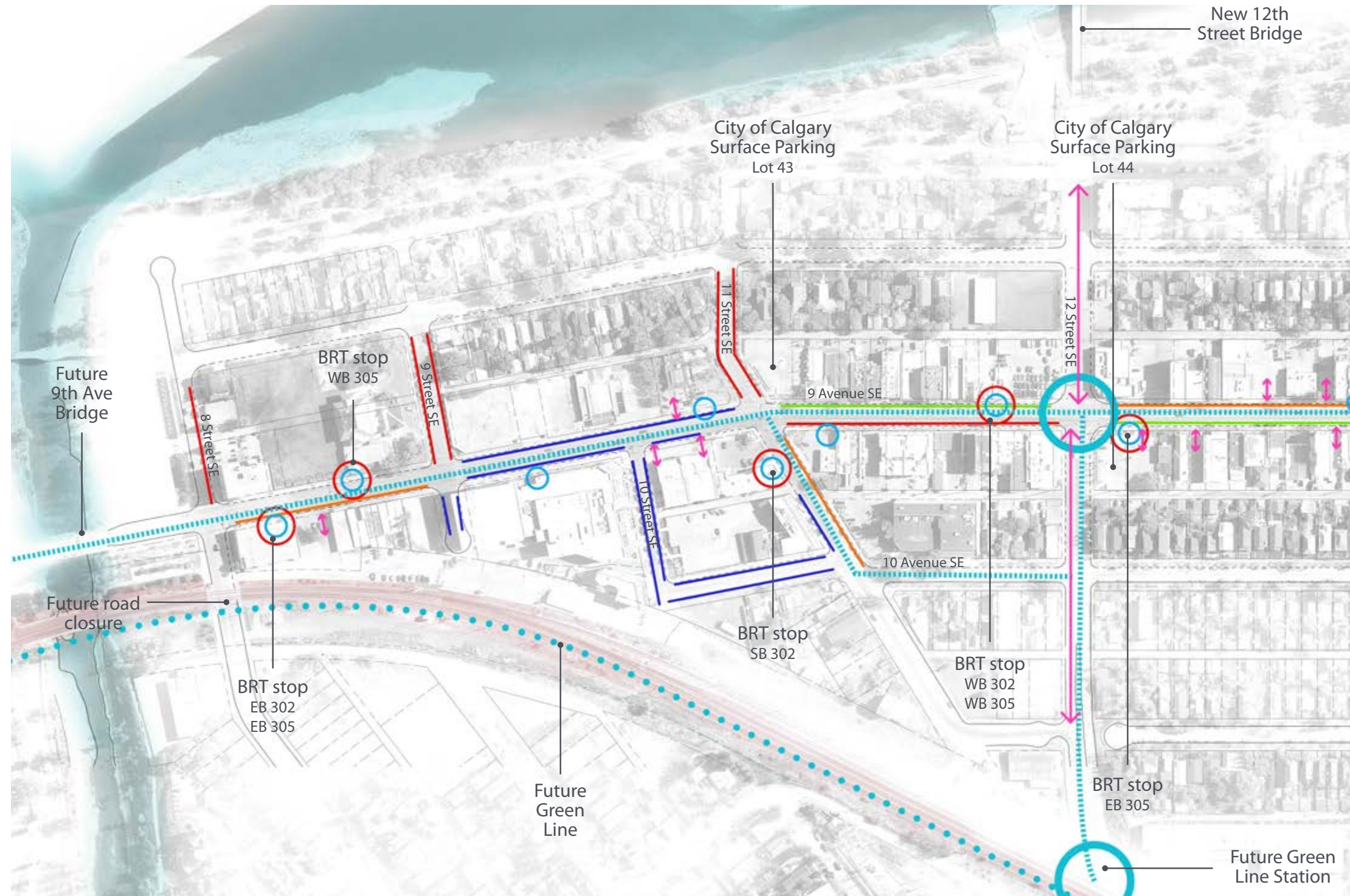
Main Streets are destination streets. These streets support retail and medium-density residential development. Pedestrians and cyclists have the highest priority on Main Streets. As a destination for visitors, they also function for the local communities. High-quality urban design and green infrastructure strategies are incorporated into Main Streets.

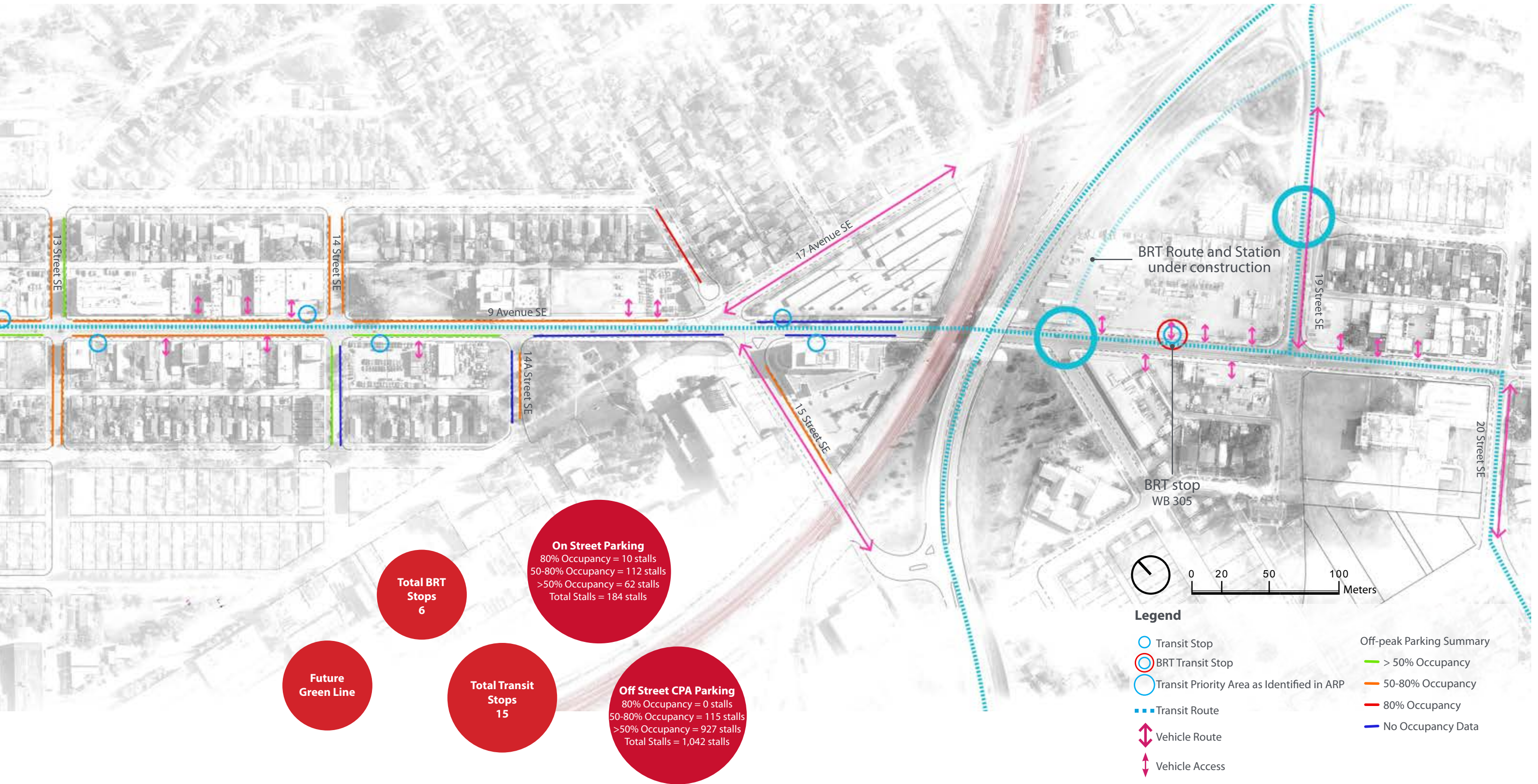
Inglewood originated as a compact community. It was walkable, streetcars along 9th Avenue provided transit options for local destinations, and CP Rail provided regional connectivity with the passenger train. Now, with the proposed Green Line alignment and S.E. BRT, Inglewood is seeing a renewed relationship with transit. Through decreased reliance on the automobile, and the implementation of transit oriented design principles, future development along the street have the potential to be pedestrian oriented. The existing street features numerous transit stops for both local routes as well as BRT service.

Currently 9th Avenue provides on-street off-peak parking in the two transit priority lanes, as well as all-day on-street parking on side streets, and two City owned off-street parking lots. From information provided by the Calgary Parking Authority the current provision of parking provides additional capacity for parking demand. The street currently has a number of vehicular access point curb cuts off of 9th Avenue to access surface parking areas.

Design Considerations

- Existing carriageway lane configuration.
- Future S.E. BRT and Green Line operations.
- Existing transit priority and level of service.
- Future 12th Street Green Line stop.
- On-street parking for local businesses.
- Vehicle access and loading from the street and alley.
- Major vehicular access points and future road closures.





Pedestrian Circulation, Cyclist Connectivity + Open Space

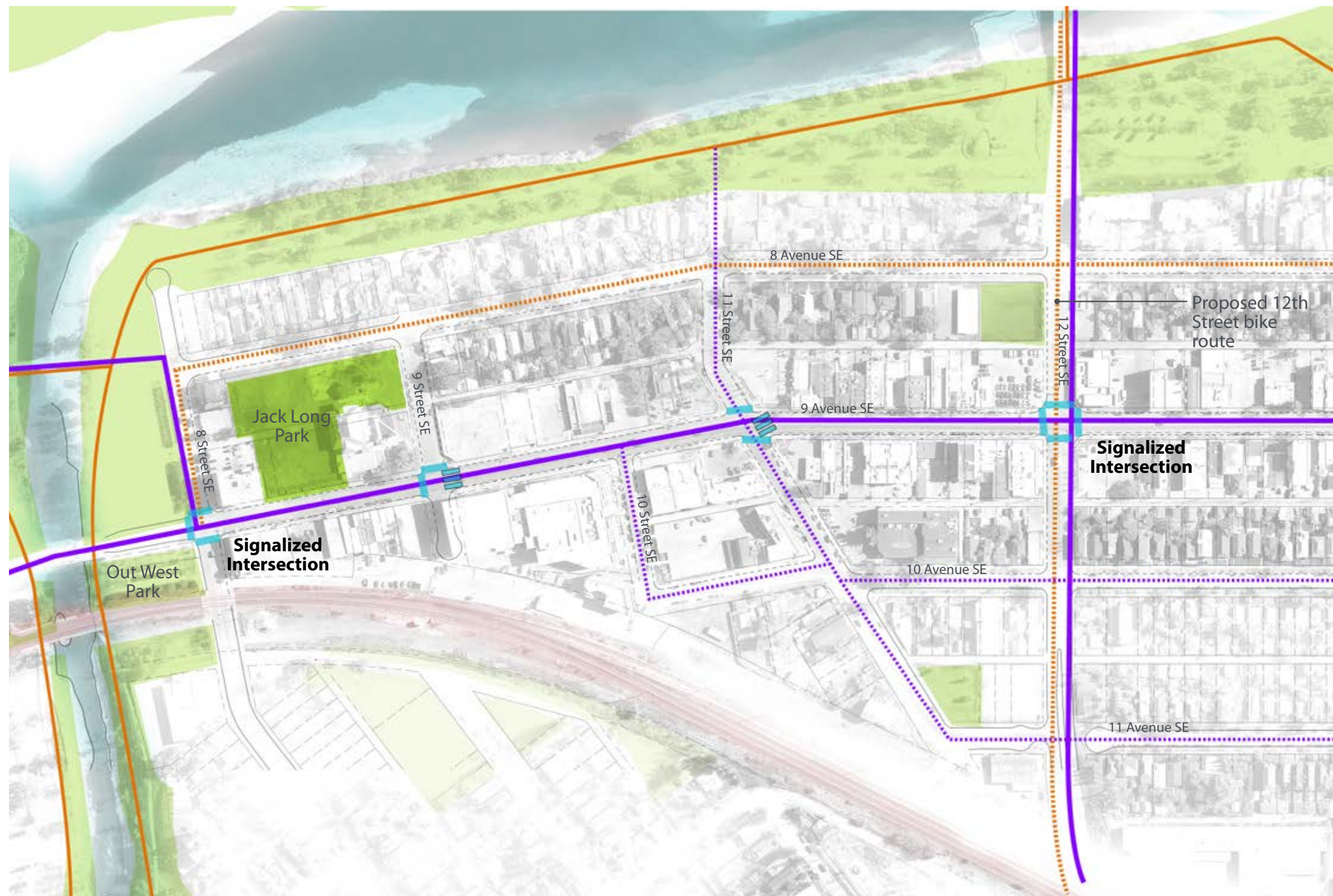
Inglewood is fortunate to be bounded by two rivers with abundant urban and natural park space including Pearce Estate Park, Inglewood Wildlands, and the Inglewood Bird Sanctuary. Connections to urban and park space can be explored through the Bow River Regional Pathway network. Community lead initiatives have seen the development of community gardens and Nellie Green Park which is well used and loved by residents and visitors alike. The 9th Avenue Streetscape study area includes the interface with (future) Jack Long Park, Mills Park, and Out West Park. Colonel Walker School fronts onto 9th Avenue and has a significant open space interface, which will influence design as does Brewery Park currently situated on private lands.

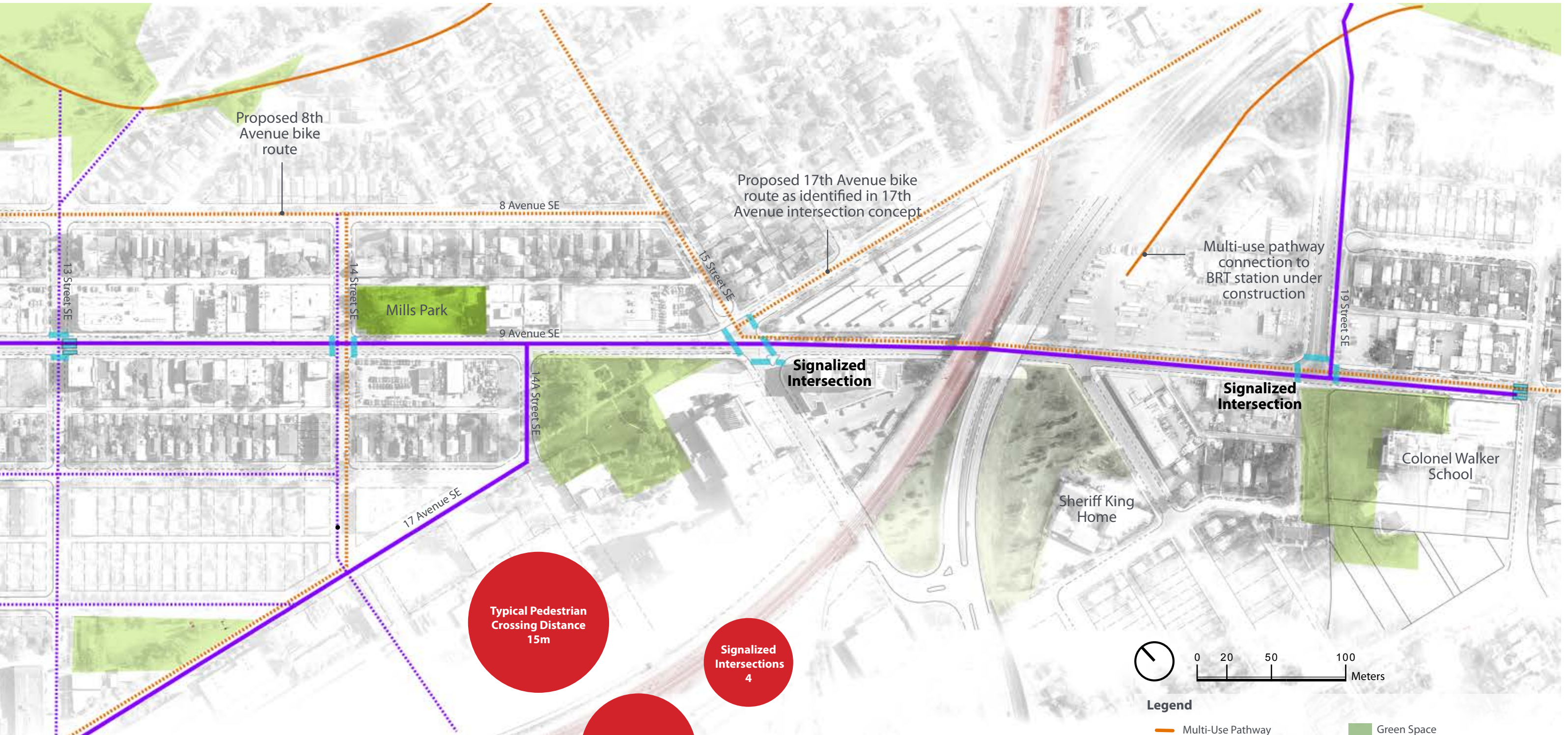
Being in such close proximity to the regional pathway system Inglewood is a great place for cycling, both commuting and recreational. Currently 9th Avenue does not function as a formal cycle route, however the parallel local street of 8th Avenue provides connectivity through the community and to the surrounding regional pathway.

In spite of the pedestrian environment along 9th Avenue it is still a primary pedestrian route with the local businesses attracting residents and visitors, local streets are also nice places for pedestrians with Inglewood character homes and tree lined streets. With limited signalized intersections, and restricted crossings pedestrian mobility is limited and has the perception of being unsafe.

Design Considerations

- Cyclist connections to the community and the surrounding bike network
- Green space and park interfaces along the street
- Public realm along 9th Avenue
- Prominent pedestrian connections and key intersections
- Future capital projects and road closures
- Alignment with mobility network outlined in the ARP

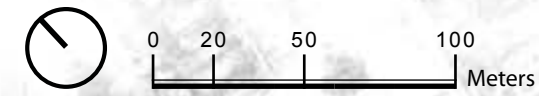




Typical Pedestrian Crossing Distance
15m

Signalized Intersections
4

Pedestrian Activated Crosswalks
3



Legend

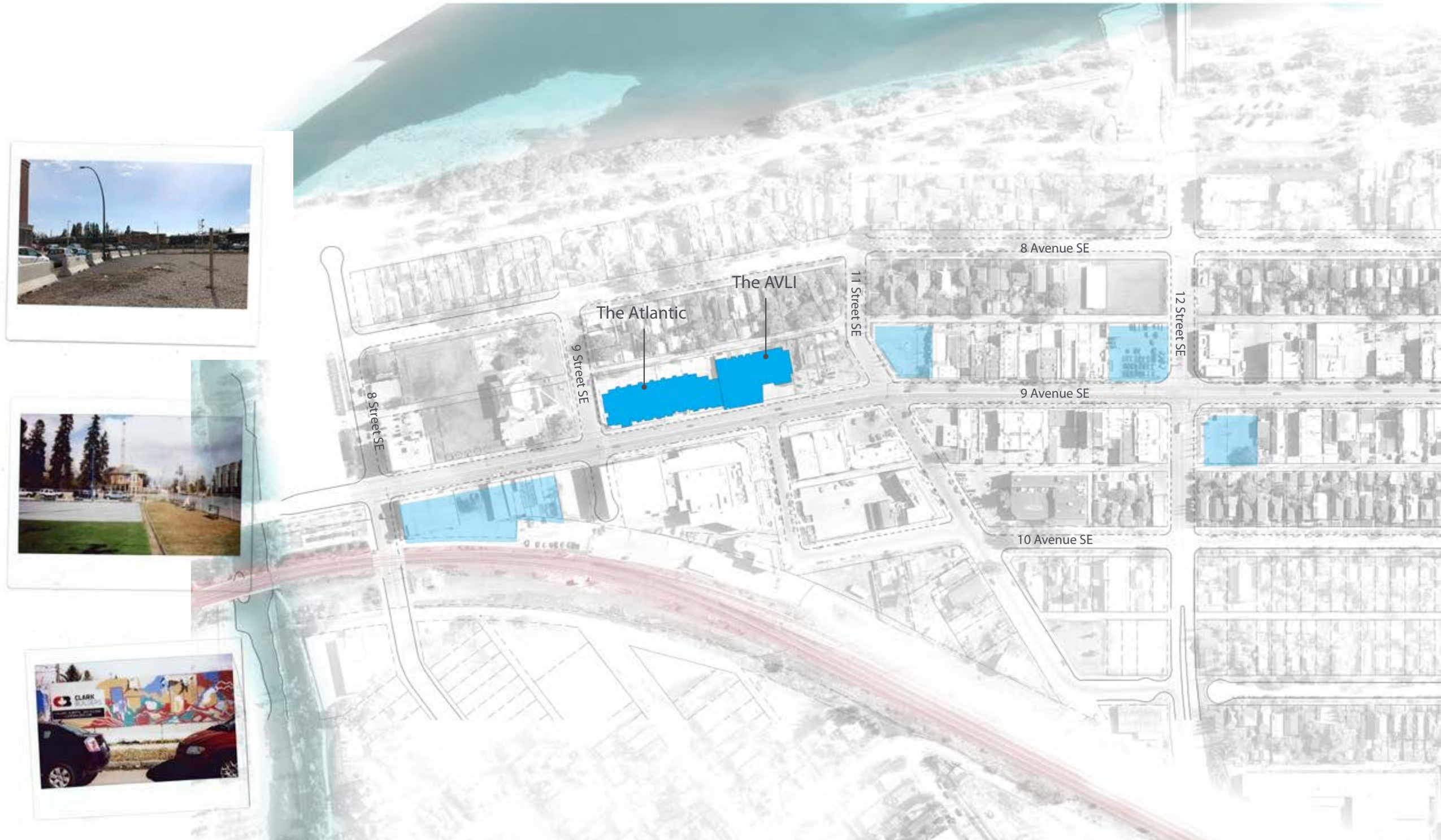
- Multi-Use Pathway
- - - Bike Route
- Major Pedestrian Route
- - - Minor Pedestrian Route
- Green Space
- ▬ Pedestrian Activated Signal
- ▬ Pedestrian Crosswalk

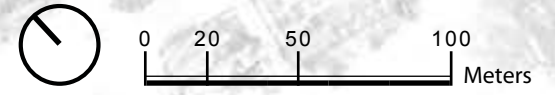
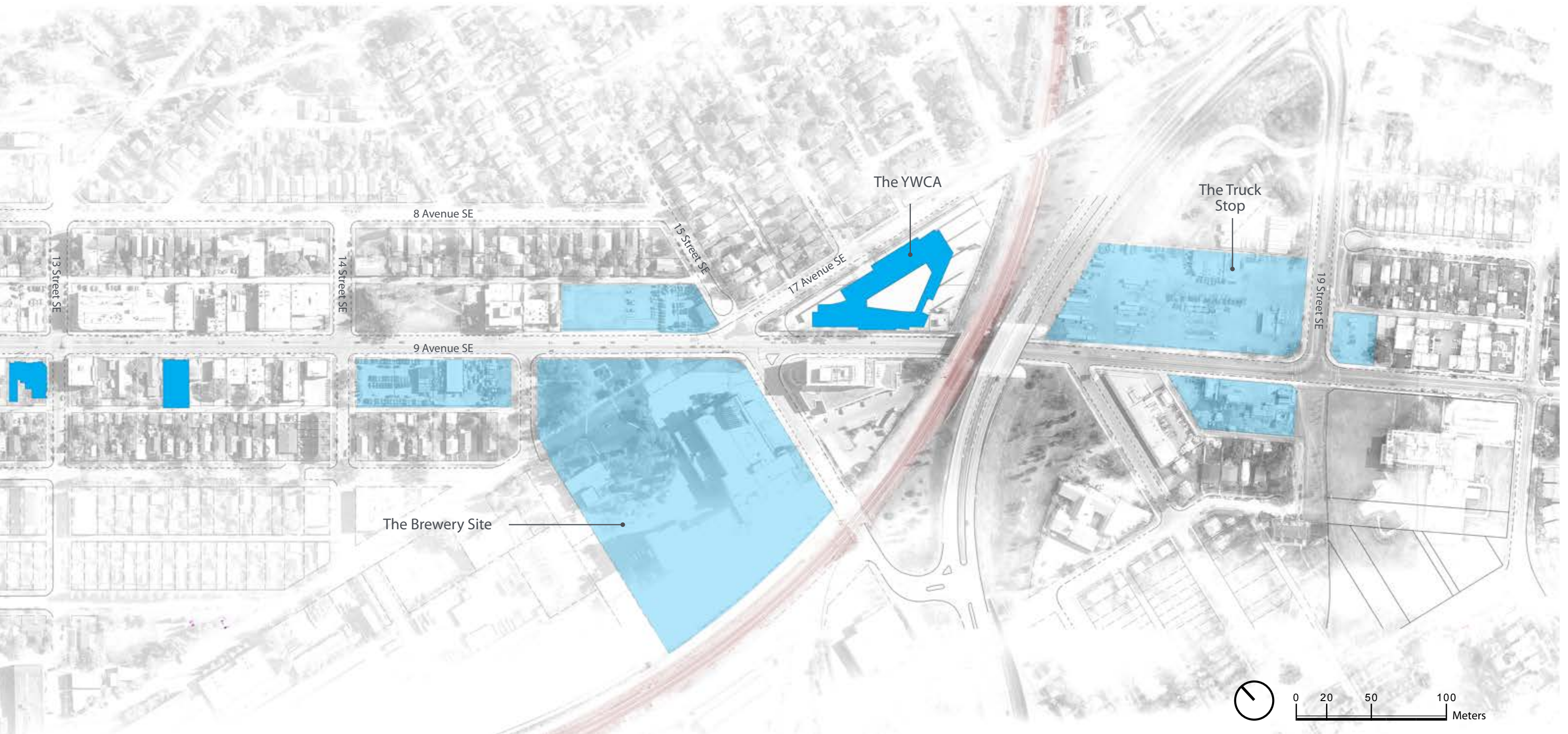
Land Use and Community Transformation

9th Avenue is a mixture of commercial, mixed-use and residential buildings that reflect the history of the community. It is a dynamic street with significant development potential and pressures. Policy identified in the DRAFT ARP provides direction for development along 9th Avenue allowing for mixed use development with a height limit of approximately six stories. Currently The City is engaging land owners and community for updates to the ARP in relation to transit oriented development opportunities for the new Green Line LRT. There are several developments underway along the Streetscape. The Molson Brewery site, YWCA, the Atlantic, the Avili and other mixed use developments will all have an impact on the character and livability of the community.

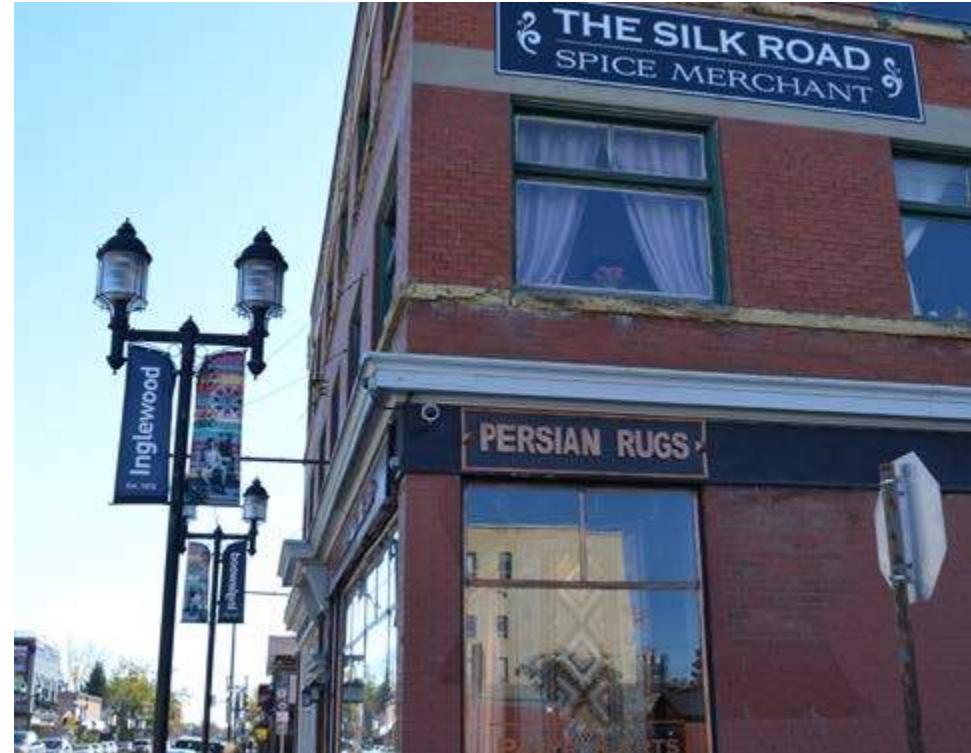
Design Considerations

- Future density along 9th Avenue with 4 active Development permits, potential build out of approx 300 new units, and additional new proposals under development.
- Additional built-in public and private parking capacity
- New commercial storefronts with 18 new CRU and over 2600sq.m commercial area
- Future development potential of vacant and under utilized sites such as the brewery site, truck stop site and Green Line station area.
- Historic building character and scale.
- Landuse, FAR and Height restrictions outlined in the DRAFT ARP
- Land assembly and ownership consolidation





- Legend**
- Active Development Permit or Under Construction
 - Main Street Development Potential



Economic Vitality + Creative Culture

Inglewood is a vibrant retail and cultural destination and is home to many established businesses, entrepreneurs, destination retail and artists. Inglewood is host to the Fringe Festival, Inglewood Sunfest, Night festival, Winter Carnival. The art scene is also alive in Inglewood and is home to Calgary Arts Development Association (CADA). Music venues such as the Blues Can, Ironwood, and Festival Hall all contribute to a vibrant music scene leading to the area being dubbed "Music Mile." With an active Community Association and BRZ Inglewood is a repository for community services and resources offering opportunities for all, regardless of age, race, economic class, or gender orientation.

There are numerous new and local restaurants along the street serving guests with diverse dining experiences and unique cuisine. With numerous local breweries and many pubs, the street provides residents and visitors with a sampling of some of the best that Calgary has to offer. Fashion and Style along the street is uniquely Inglewood with boutique retailers as well as manufactures calling the community home. Although there is a strong retail presence, 9th Avenue is thriving with professional services including architecture firms, marketing companies, tech start ups and, industrial and engineering services. 9th Avenue has the potential to facilitate and foster the growth and development of this creative ecosystem, and these professional services will in-turn support retail and hospitality.

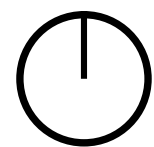


Inglewood is a **GROWING ECONOMIC + CREATIVE ECOSYSTEM**



“.....I thought how similar a vibrant, human scale street is to good jazz. It is a conversation, with spontaneity and structure, ego and humility, noise and intense calm....”

Rebecca O'Brien, Inglewood BIA



3. Design Concept

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Transit, Vehicular Circulation, Access + Parking

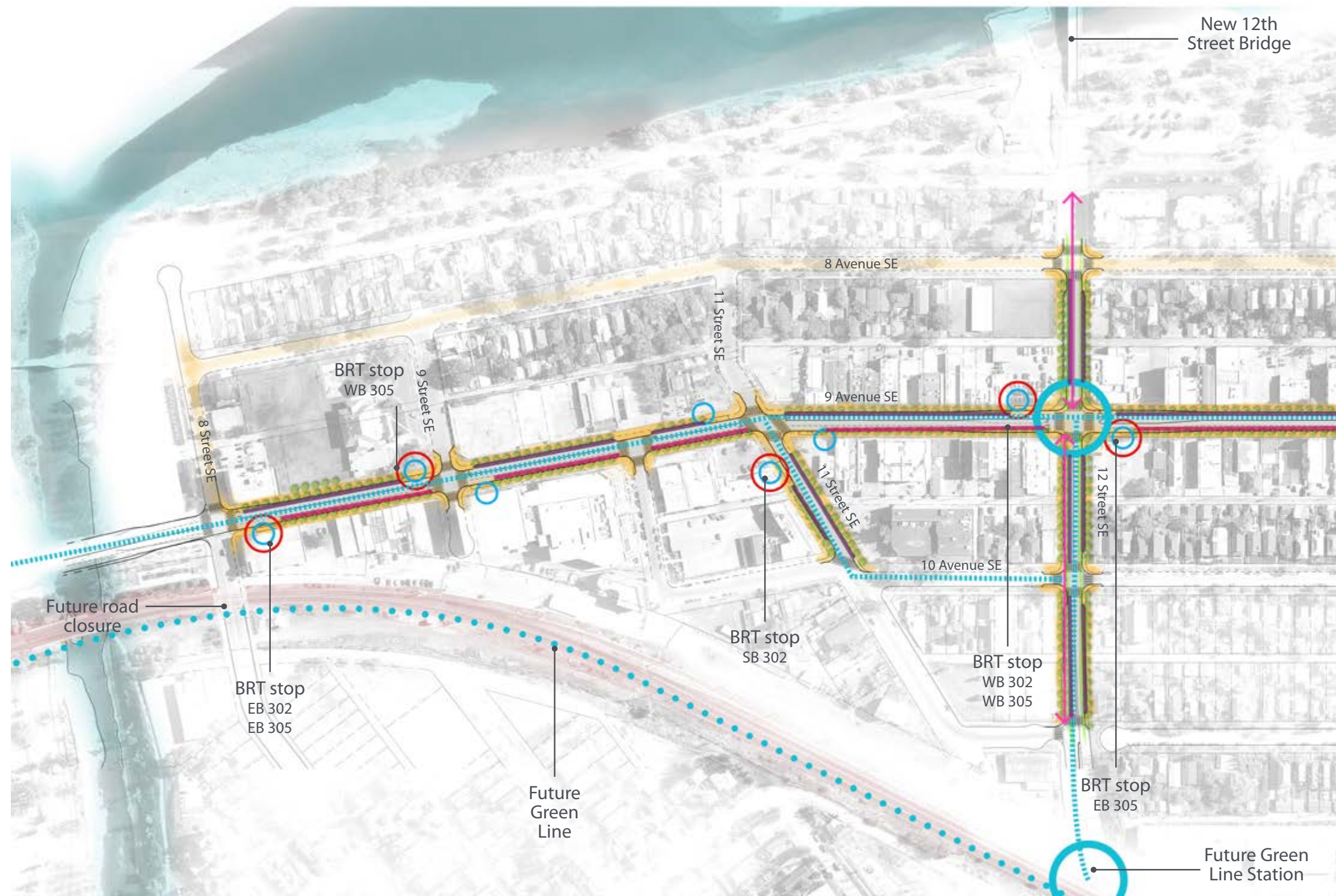
The Master Plan concept proposes a shift for 9th Avenue away from vehicular-oriented towards a traffic calmed, pedestrian oriented street. The concept prioritizes transit along 9th Avenue by providing an east-bound P.M. peak priority bus lane, as well as integrating both regular and BRT transit stops in to the streetscape design. Transit priority areas are identified in alignment with the DRAFT ARP. Both permanent and off-peak parking is provided to generally meet the same volume of on-street parking currently being provided. Access to private developments and off-street parking areas will require further analysis. However, preference will be to provide access from side streets and lanes.

Design Strategies

- Eastbound transit priority lane with on-street transit stops.
- West bound shared transit lane with both lay-by and on-street stops and queue jumping areas. Queue jumping to be explored further in detailed design.
- Streetscape improvements along 12th Street to better connect future Green Line Station to 9th Avenue and the community.
- Consideration for future 17th Avenue BRT alignment east of Blackfoot Trail.
- Future consideration for loading zones and access along 9th Avenue.
- Permanent parking on north side of street
- Off-peak parking on south side of street

Note:

- The proposed design of the Main Street should not conflict with the under construction BRT project from 16 St to Blackfoot Tr, on 9 Ave and 19 St SE.

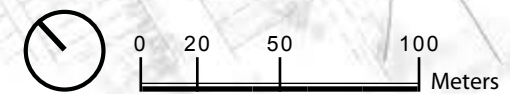




Future
Green Line
Connection

Total BRT
Stops
6

Total Transit
Stops
15



Legend

- Transit Stop
- BRT Transit Stop
- Transit Priority Area as Identified in ARP
- - - Transit Route
- ↕ Vehicle Route
- Off Peak Parking
- Permanent Parking

Parking Summary

Street Cross Sections

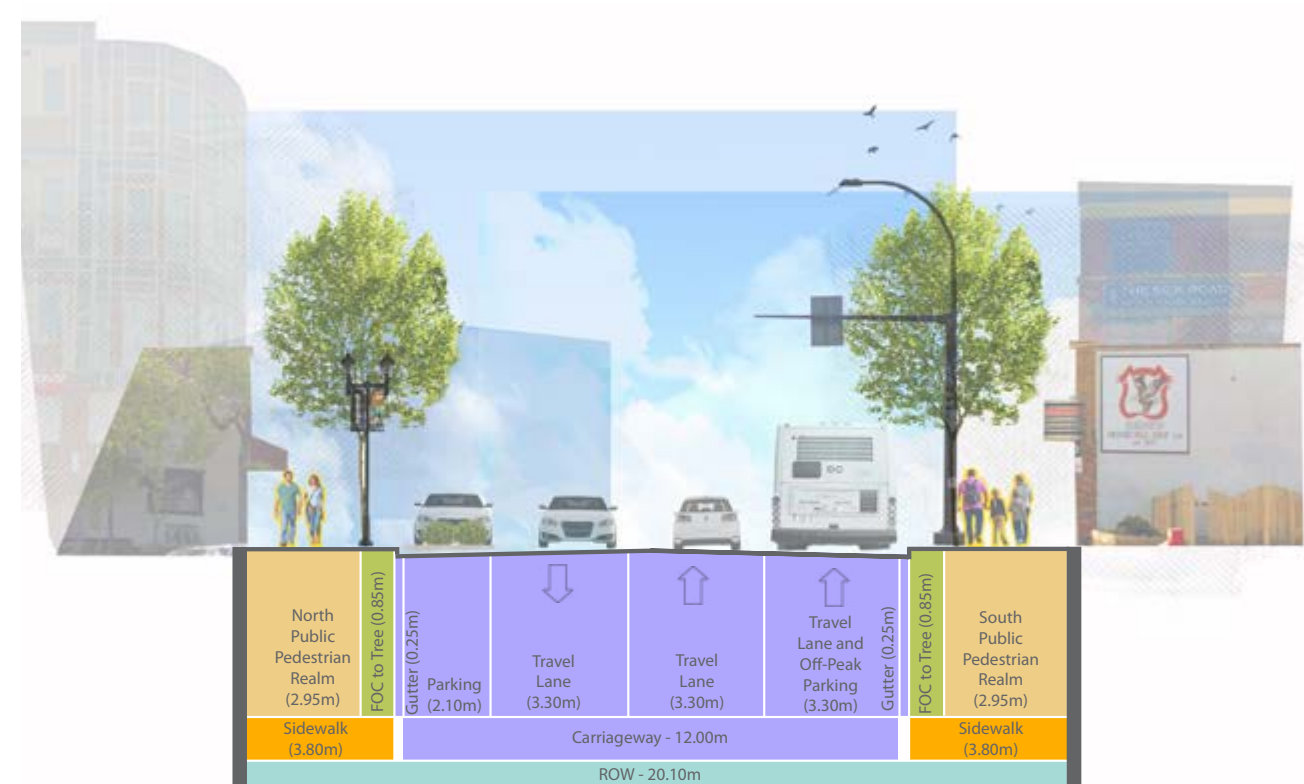
The proposed street cross sections were selected to increase the area of pedestrian realm while providing transit priority and off-peak parking lanes, drive lanes, and permanent parking lanes. With a consistent right-of-way of 20.10m, space allocation is prioritized for the pedestrian and for public realm improvements, which will provide additional opportunities for businesses in the form of patio areas, and for pedestrian in the way of a more comfortable street environment. The provision of corner bulbs and flexible permanent parking areas provide the community with opportunities for gathering and socializing. On-street parking is provided to meet the current provision in the existing conditions.

The typical cross section will vary slightly along 9th Avenue to account for more recent streetscape improvement areas and unique context locations. The laneing configuration changes between 19th Street to 20th Street SE as this segment of street currently operates with two drive lanes. This configuration will be explored further in detailed design.

A percentage of right-of-way area is provided to illustrate space allocation and prioritization along the street. This percentage incorporates an average of the potential area for corner bulb, flexible parking area for patios and sidewalk space to make up the pedestrian realm.

9th Avenue - Typical Street Cross Section

The section assumes a 12.00 metre carriageway, with the additional area being dedicated to increase the potential of the pedestrian realm.



Design Strategies:

- Two travel lanes
- One transit lane + Off-Peak parking
- One lane parking
- Wider sidewalks
- Extensions at corners + Transit stops along north side
- Dedicated transit lane to accommodate bike traffic



46% Pedestrian

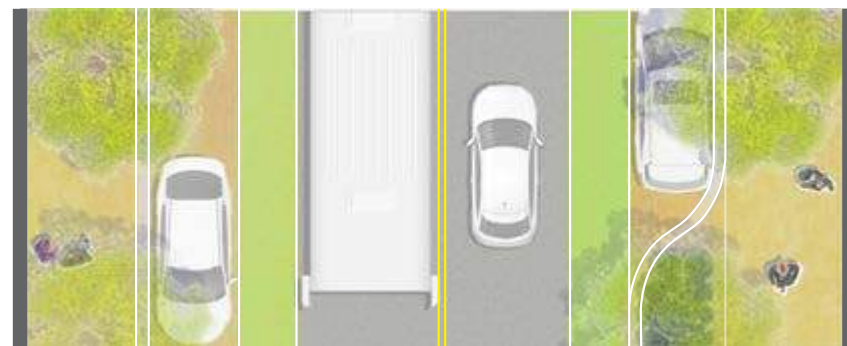
54% Vehicular

28% Existing Pedestrian

72% Existing Vehicular

12th Street - Typical Street Cross Section

The section assumes a 13.80 metre carriageway, including a provision for bike facility, with the additional area being dedicated to increase the potential of the pedestrian realm.



Design Strategies:

- Two travel lanes
- Two permanent parking lanes
- Bike facility
- Wider sidewalks
- Extensions at corners + Transit stops

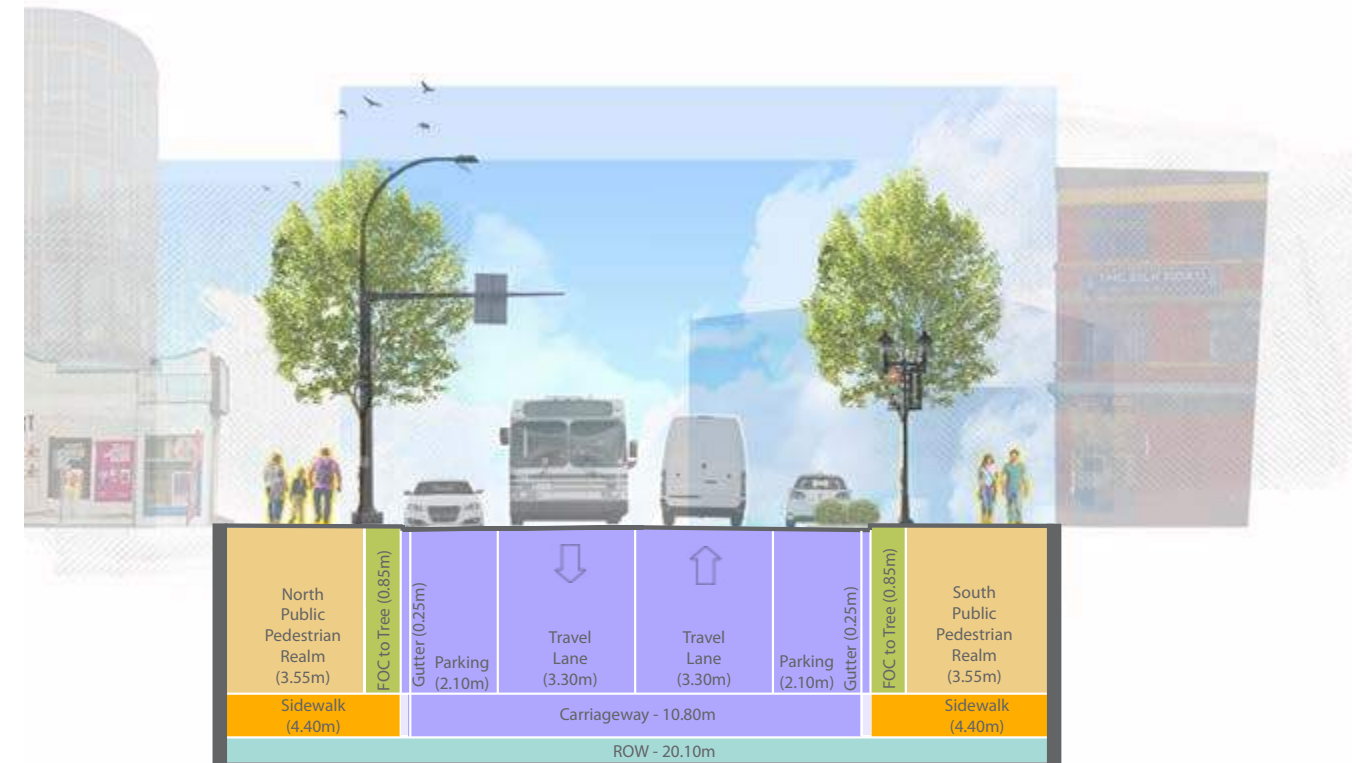


Note:

- This proposed cross section and bike facility will be assessed further in detailed design

9th Avenue - 19th Street to 20th Street Cross Section

The section assumes a 10.80 metre carriageway, with the additional area being dedicated to increase the potential of the pedestrian realm.



Design Strategies:

- Two travel lanes
- Two permanent parking lanes
- Wider sidewalks
- Extensions at corners + Transit stops

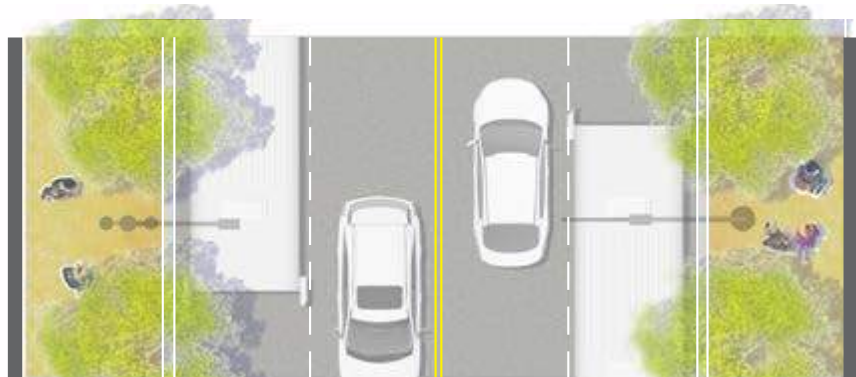
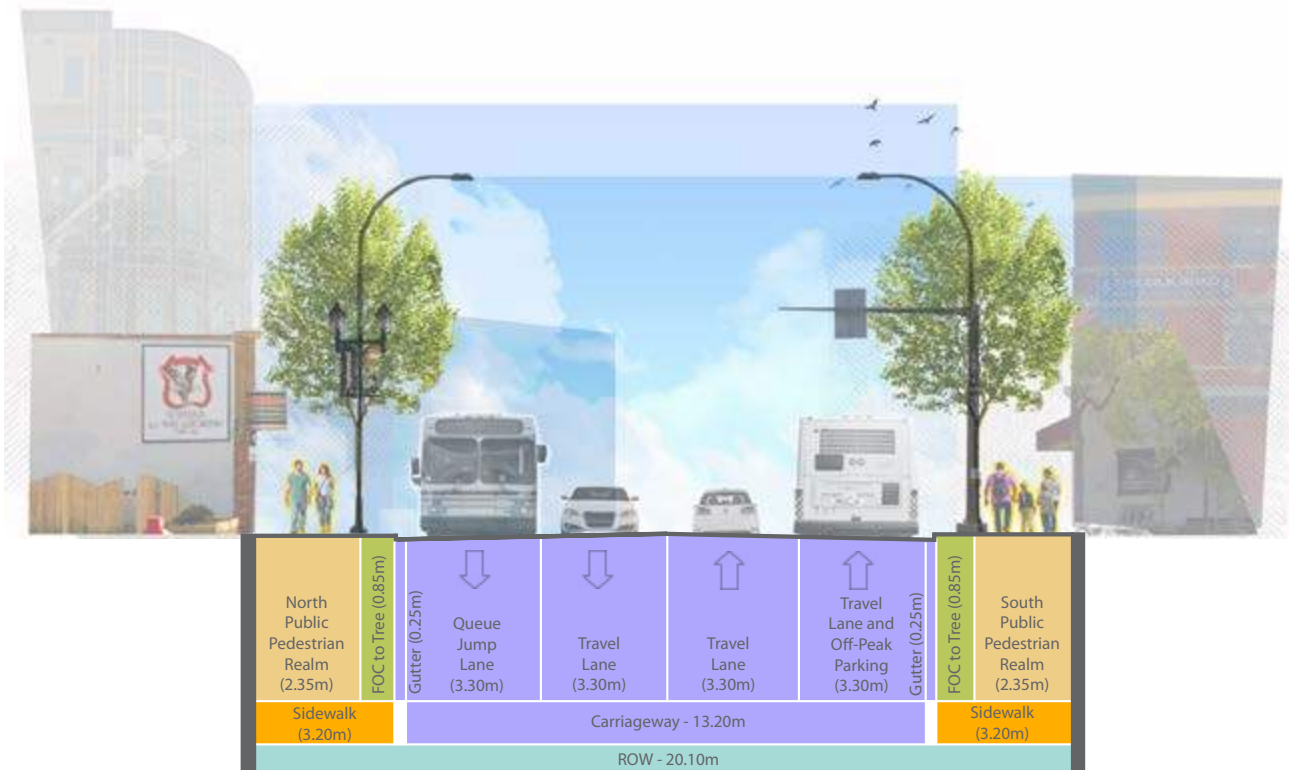


Note:

- This proposed cross section will be assessed further in detailed design

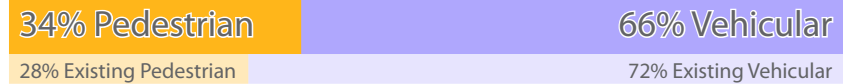
9th Avenue - Typical Queue Jump Section

The section assumes a 13.20 metre carriageway, with an additional queue jump lane on the North side located at specific intersections.



Note:

- Queue jump lanes and intersection configuration will be explored further in detailed design.



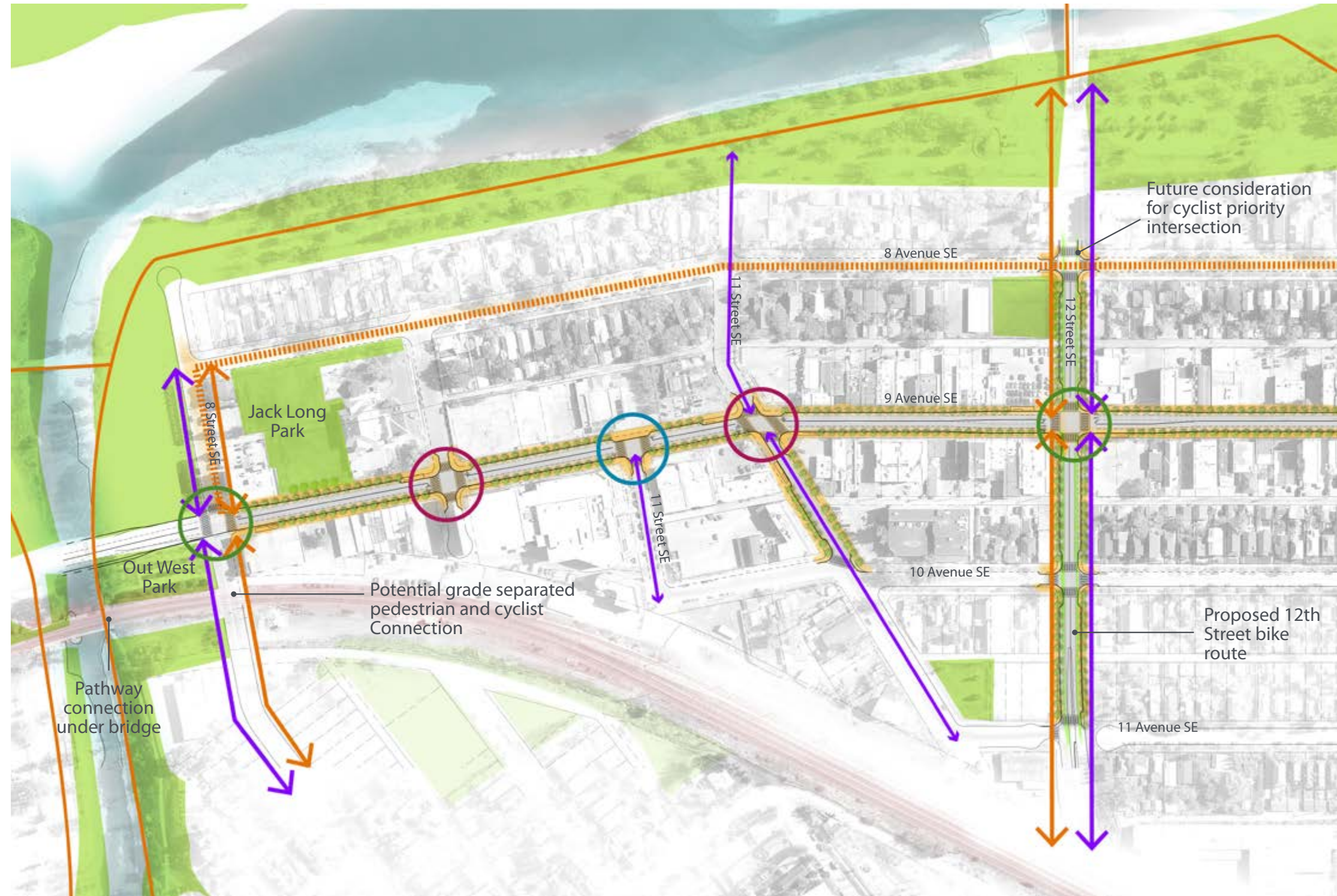
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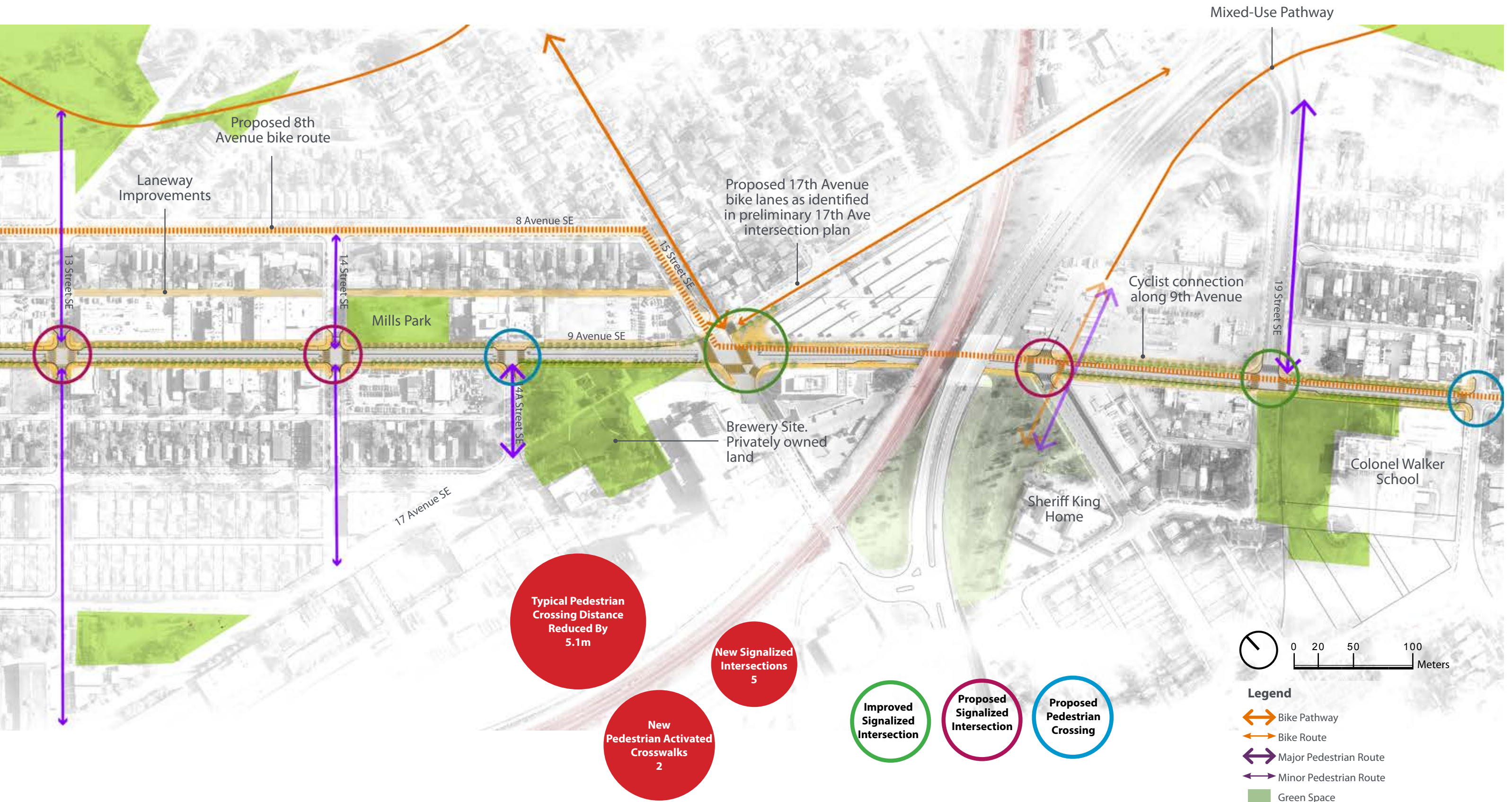
Pedestrian Circulation, Cyclist Connectivity + Open Space

The plan for 9th Avenue prioritizes pedestrians by allocating additional space of the right-of-way to sidewalk areas. Intersection improvements will provide both signalized and pedestrian activated crossings to facilitate pedestrian movement throughout the network. Cyclist mobility is provided along 8th Avenue, which is already well-used by cyclists. Modifications to 8th Avenue may be required to implement an on-street bikeway that is accessible to all ages and abilities, or AAA rated. Key cyclist priority connections are required to connect the 8th Avenue route to 9th Avenue and the surrounding regional pathway network. Open space along 9th Avenue is integrated into the streetscape design while public realm improvements and improved pedestrian and cyclist mobility will help connect to the surrounding open space in the community.

Design Strategies

- Cyclist priority connection along 8th Avenue.
- Improved public realm for pedestrians along 9th Avenue.
- Increased signalized intersections to improve mobility and safety for pedestrians.
- Integrated park areas along 9th Avenue.





Design Concept - 8th Street to 12th Street SE

Anchored by the Elbow River and the future 9th Avenue bridge this area acts as a gateway to the community and to downtown. With a potential future pedestrian and cyclist connection along 8th Street and a focal intersection, a strong visual gateway is provided. The streetscape will integrate with the edge of Jack Long Park creating a pedestrian focused interface. Side streets offer connections to the community and surrounding areas, while 12th Street is re-imagined as a multi-modal connection to the new 12th Street bridge and future 12th street Green Line station.

8th Street: Making Place for the Community

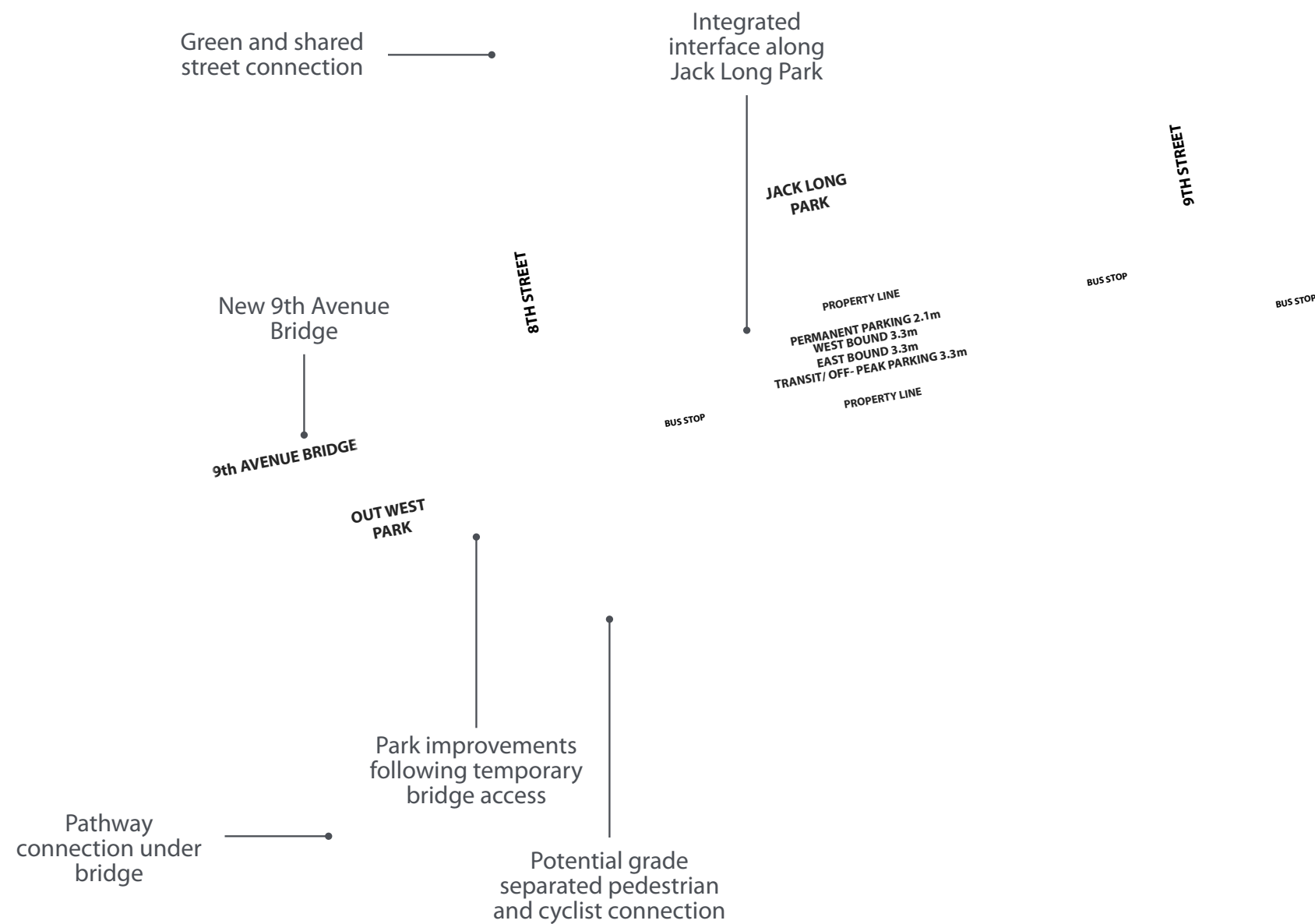
8th Street SE is a distinct entry to 9th Avenue and a critical connection to the community of Ramsay. As a gateway that speaks to the context of the railway, the river and the community this is a unique place for people and a pedestrian focused public realm. With the future closure of the at-grade CP rail crossing on 8th Street and future Green Line works, 8th Street will be re-imagined as a safe and focal connection and amenity for the community. The area will feature a prominent pedestrian and cyclist connection that will speak to the intersection of Ramsay and Inglewood, as well as the rivers and the railway, both new and old. A distinct crossing will be provided at 9th with the potential for a future green and shared street along 8th to the north of 9th Avenue.

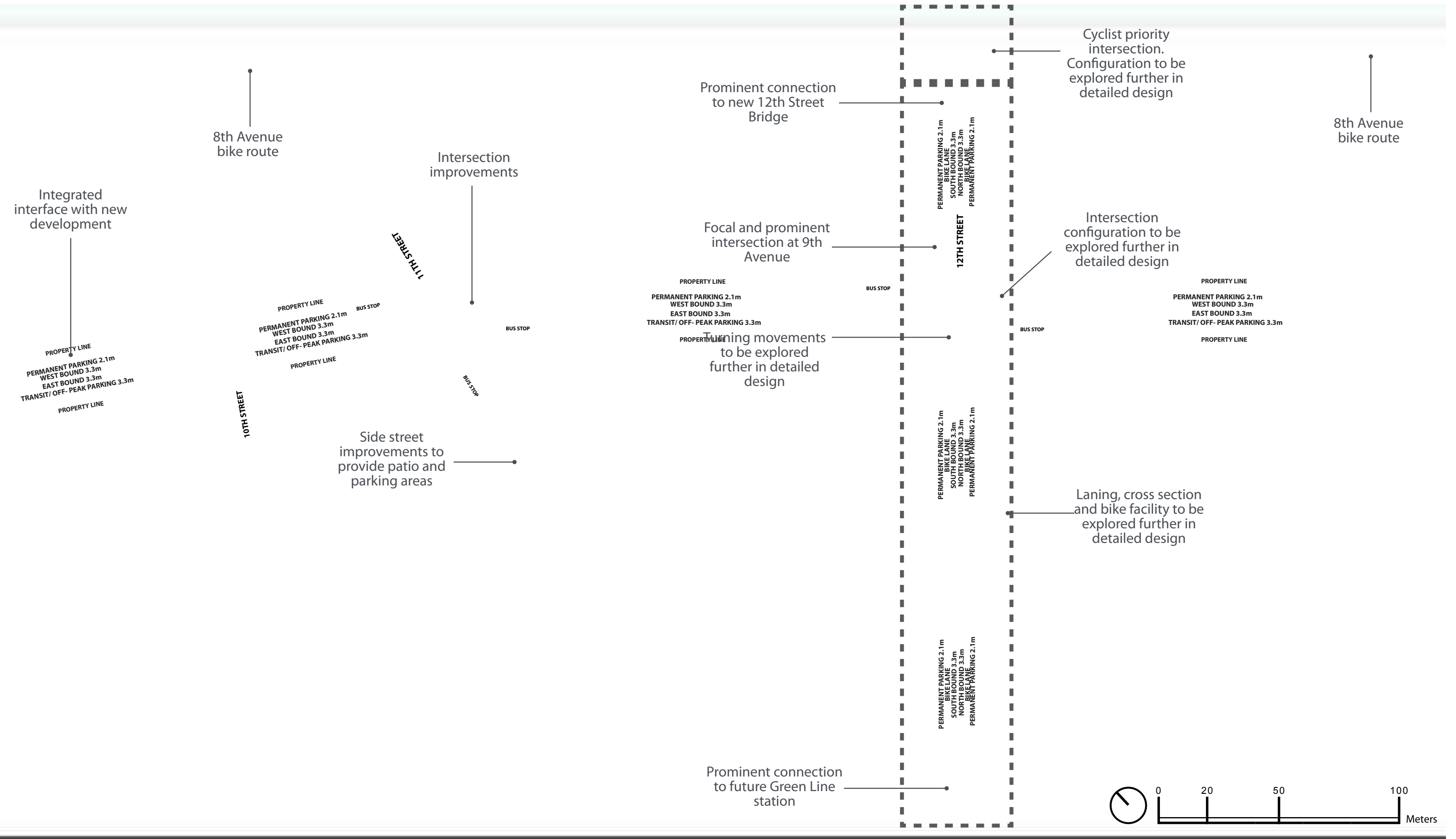
12th Street: Connecting the Community

The intersection of 12th Street and 9th Avenue is a hub for the community. Distinct and prominent corners let visitors know that they have arrived. With improved connections to the river pathway system and the Zoo to the north and the future 12th Street Green Line station to the south 12th serves as a multi-modal street connecting the community.

Design Strategies

- Bike facility on 12th Street.
- New provision for permanent parking along 12th Street.
- Strong connection to 12th Street bridge and river pathway system.
- Future connection to the 12th Street Green Line station.
- Prominent and focal landing for cyclists and pedestrians at 9th Avenue intersection.
- Reduce pedestrian crossing distance at intersections.
- Additional pedestrian crossings along 12th Street





Design Concept - 12th Street to 15th Street SE

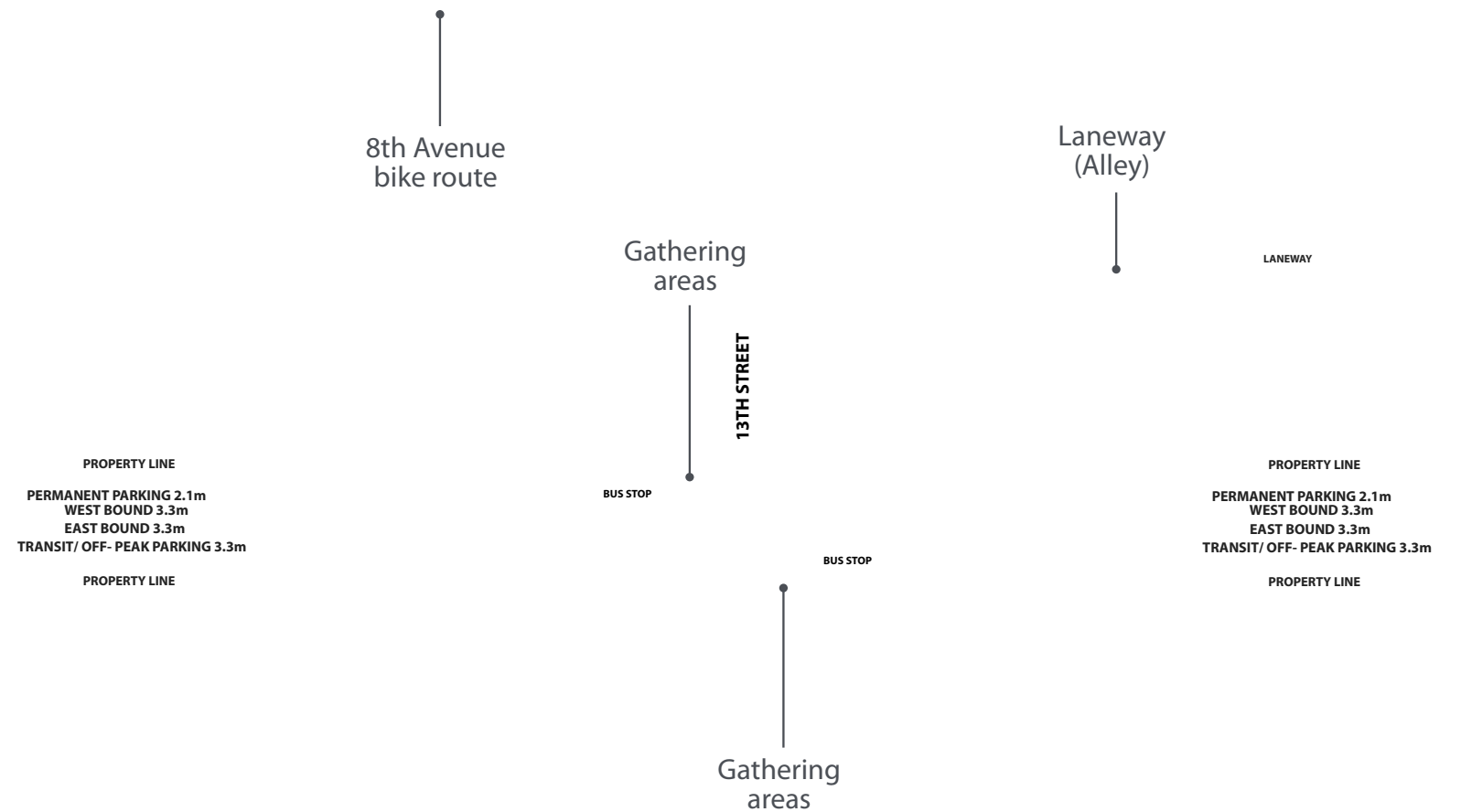
The central segment of 9th Avenue features a distinct mix of adjacent land uses ranging from historic buildings, park areas, new development, and surface parking and service lots. Laneway improvements will be provided in the lanes north of 9th between 15th Street and 13th Street. This will provide an added amenity for the community and integrate locations like Mills Park and the Stewart Barn. The interface along Mills Park will be improved to better integrate the redesigned park improvements to the street. The intersection at 17th Avenue can be re-imagined to create a gateway and a needed community amenity space. Cycle connections will be prioritized to connect to the 8th Avenue cycle route.

17th Avenue: New Opportunities for the Community

The intersection at 17th Avenue and 9th Avenue is a critical connection between East and West and signifies a sense of arrival to 9th Avenue. With a pedestrian focused intersection that accommodates the movement of cyclists, transit and vehicles this is a multi-modal connection. The intersection integrates the evolving surrounding land uses and development and creates meaningful space for community and public gathering.

Design Strategies

- Reduce pedestrian crossing distance.
- Pedestrian oriented public realm.
- Creation of substantial areas for public and community gathering.
- Prominent cycle connection to 8th Avenue and 17th Avenue.
- Creation of a focal intersection, fitting with the proposed character for 9th Avenue.
- Minimize conflicts with utilities.
- Integrated transit area and requirements for efficient movement.
- Better connections for the community.
- Improved sight lines.



Intersection configuration to be explored further in detailed design. Should Main Streets implementation occur, The City will evaluate funding availability for reconfiguration of the intersection. If it is not available, then reconfiguration may occur at a later date

Prominent cycle connection to 9th Avenue

Integrated interface with new Mills Park

14TH STREET

MILLS PARK

LANEWAY

BUS STOP

BUS STOP

PROPERTY LINE
PERMANENT PARKING 2.1m
WEST BOUND 3.3m
EAST BOUND 3.3m
TRANSIT/ OFF- PEAK PARKING 3.3m
PROPERTY LINE

14A STREET

PROPERTY LINE
PERMANENT PARKING 2.1m
WEST BOUND 3.3m
EAST BOUND 3.3m
TRANSIT/ OFF- PEAK PARKING 3.3m
PROPERTY LINE

Access provision

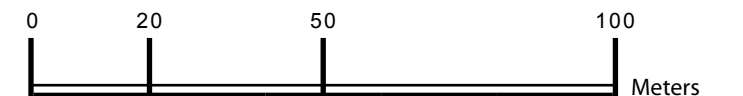
TRANSIT AREA

17TH AVENUE

15TH STREET

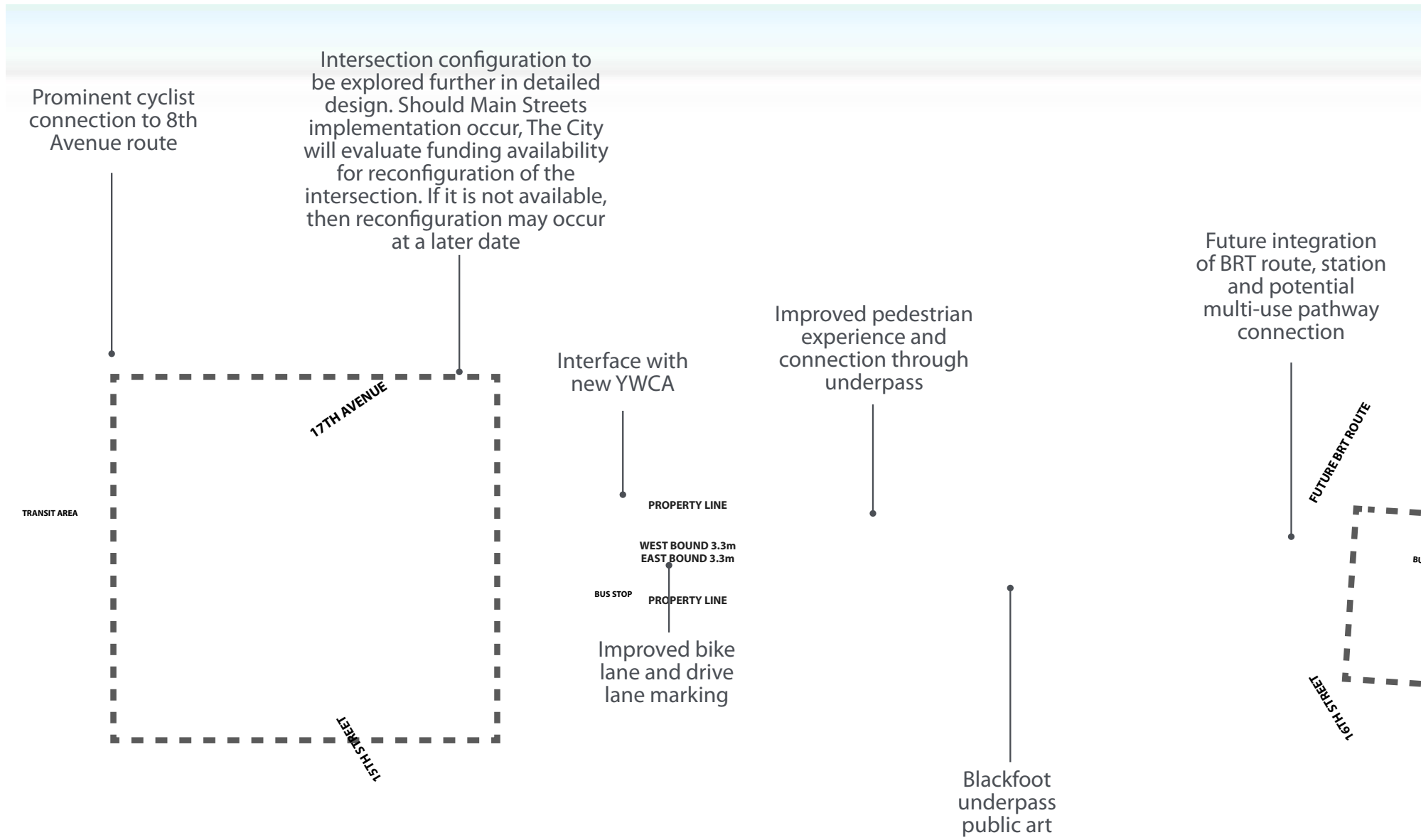
Gateway and community amenity area

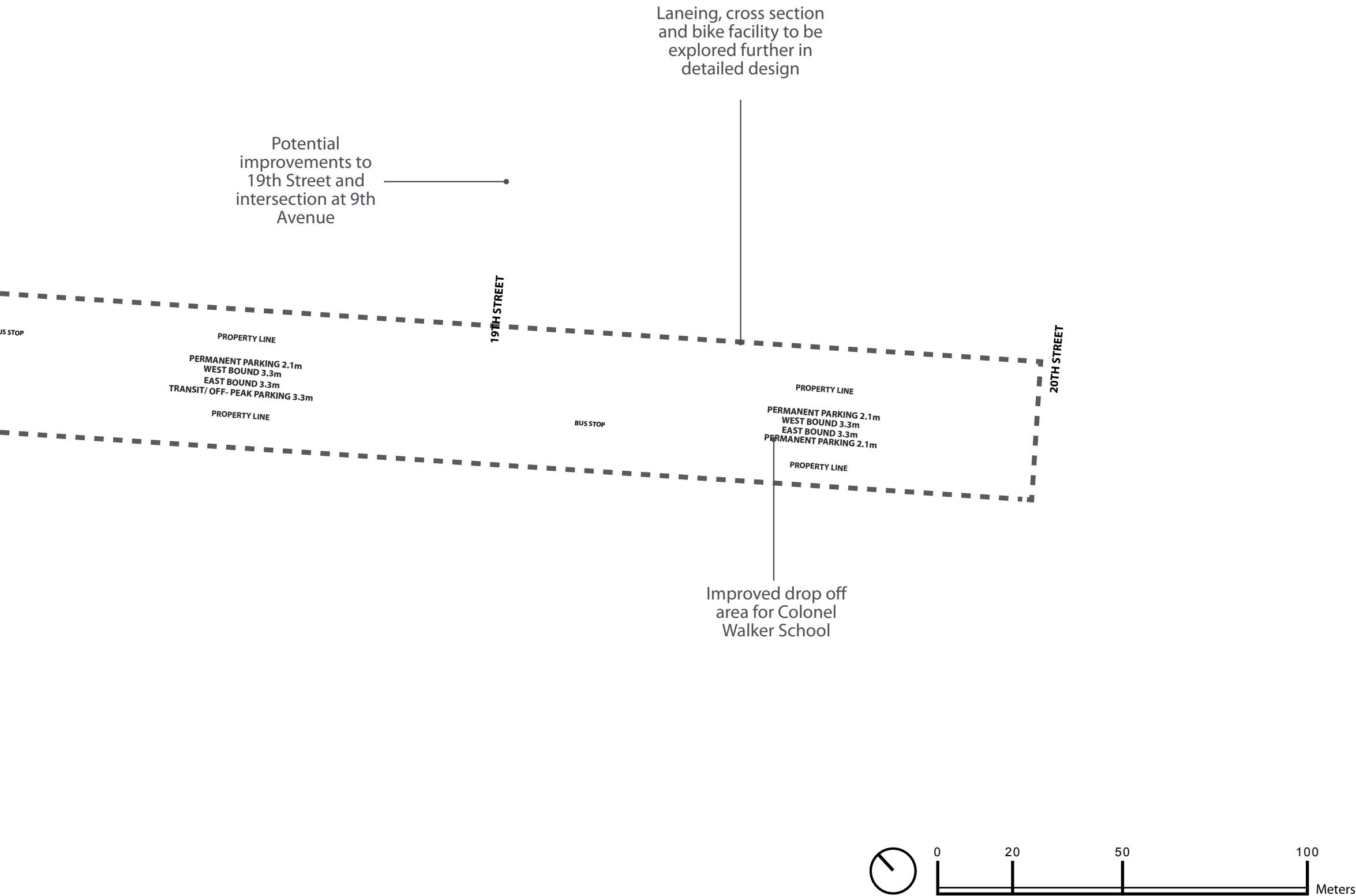
PROPERTY LINE
WEST BOUND 3.3m
EAST BOUND 3.3m
BUS STOP
PROPERTY LINE



Design Concept - 12th Street to 15th Street SE

This area provides a needed link to connect the community and 9th Avenue, east to west through the Blackfoot underpass. Public art provided on the underside of the Blackfoot Trail underpass provides a distinct and memorable experience for pedestrians, cyclist, and vehicles. This will attract people and better connect the community as well as users and employees of the YWCA, Sheriff King Home and Colonel Walker School. Future BRT connections and development in the truck stop site will change the fabric of the area, the streetscape design can be extended along 19th Street and integrate BRT station areas. Streetscape improvements between 19th and 20th Street offer a generous public realm and side walk area with on-street parking that will provide improved drop off in front of Colonel Walker School and improved pedestrian connection into the community.



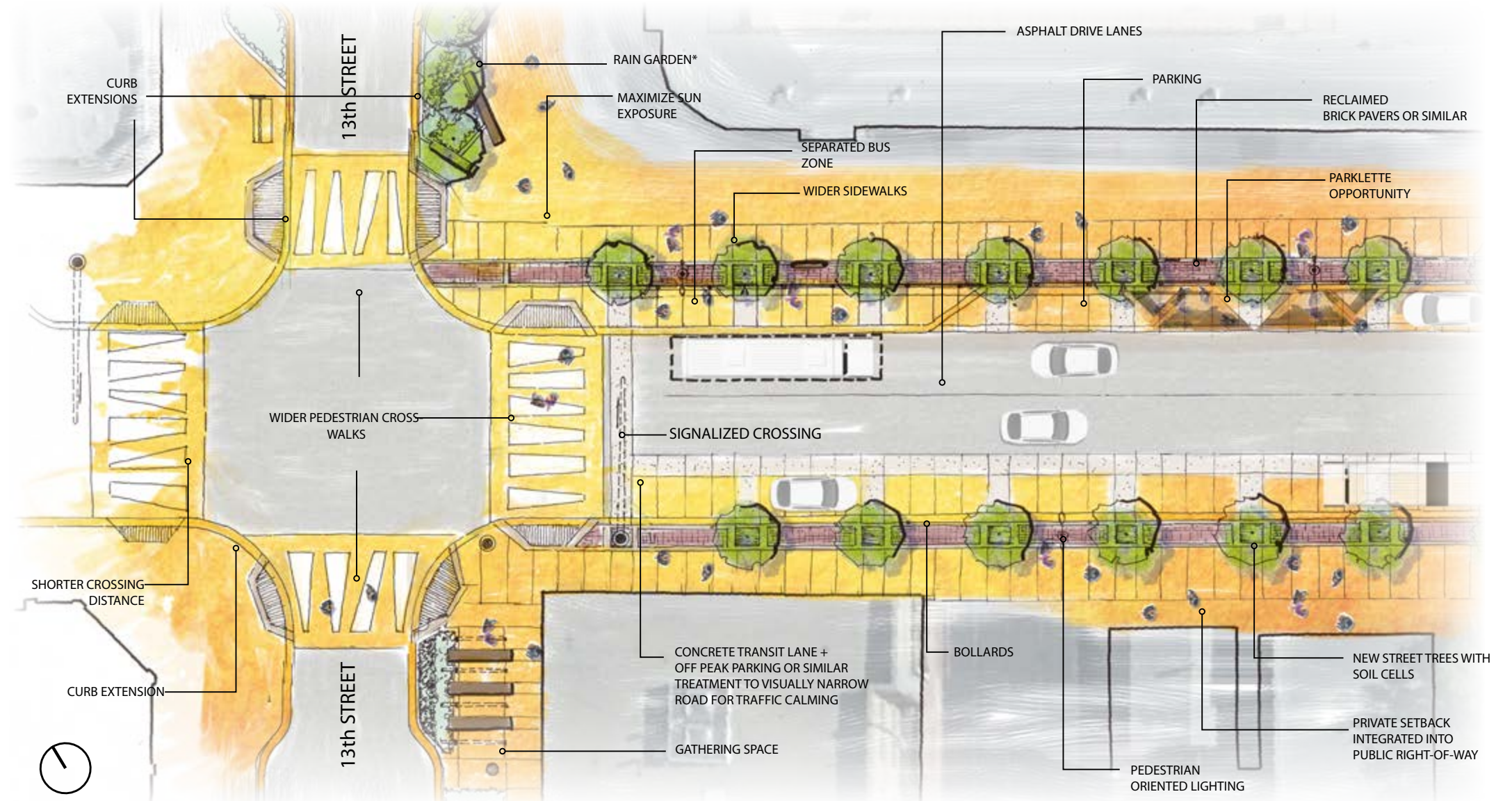


Creating a Unique Street Ecosystem

9th Avenue will reflect the unique and eclectic character of Inglewood. The street will be pedestrian oriented and provide opportunities to foster and grow Inglewood's creative and innovative culture. The street will offer a slow paced environment where people can pause, and engage in meaningful interaction. 9th Avenue will be the living room for local businesses, and provide a diverse range of community gathering opportunities. The street will allow for efficient transit movement while also promoting Inglewood as a vibrant and unique destination.

Design Strategies

- Increased public realm area allocation in ROW
- Increased area on north side to maximize sun exposure
- De-cluttered street furnishings to limit pedestrian obstructions
- Improved tree canopy and health
- Linear street tree planting and a typical 6.7m rhythm
- Clustered planting areas
- Continuous tree trench for increased soil volume and stormwater management
- Private setback interface
- Context specific interventions in setback area
- Continuity in materials between public realm and private setback area
- Eclectic furniture zone module
- Banding that frames area for material change
- Flexible internal component (bricks, planting, art)
- Material change for accessibility
- Visual extension
- Material connection across transit lanes
- At grade transition for flexibility
- Intuitive parking location based on street rhythm
- Flexible and adaptable
- Specific areas can evolve as the community grows

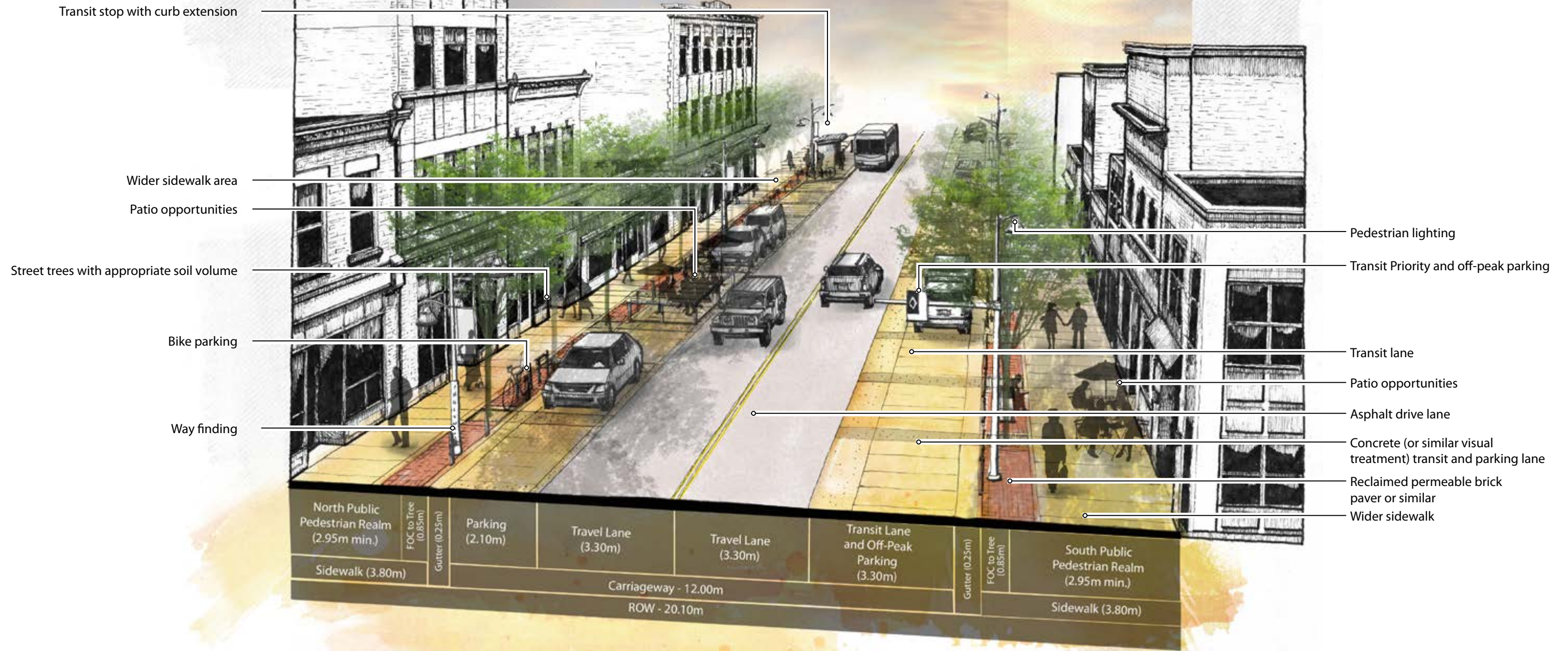


DETAILED CONCEPT FOR 9TH AVENUE

Note:

- Should Main Streets implementation occur, The City will evaluate funding availability for long-term maintenance of rain gardens.
- Pedestrian scale lighting may require separate electrical metering and be paid for by a third party.
- Pedestrian scale lighting spacing will be consistent with recent streetscape improvement blocks, and be a standard product currently used by The City.
- Street lighting, tree alignment and location will be reviewed in detailed design.
- Discussion with utility providers are underway, further coordination will be provided in detailed design.
- Areas for patio opportunities will be explored further in detailed design.

BIRDS EYE PERSPECTIVE LOOKING EAST



Place Making

Laneways (Alleys): Building on Community Assets

Laneways (alleys) are an under-utilized aspect of the community. Laneways can be developed to contribute to the evolution of 9th Avenue and connect the community. Laneways can provide an amenity and serve as additional community gathering areas, while also being an integrated aspect of the surrounding environment.



EXISTING



CONCEPT FOR ALLEY BEHIND STEWART BARN

Design Strategies

- Opportunities for community gathering.
- Paved lanes with potential planting opportunities.
- Seasonal lighting.
- Food truck parking locations.
- Public art opportunities.
- Public/Private Partnerships with adjacent land-use.

Note:

- Legislation governing the use of lanes/alleys will be given consideration during subsequent project phases.

Crosswalks & Street Corners: More than Crossing a Road

Street corners should slow vehicle movement prioritizing pedestrians along the street. It's proposed to provide pedestrian focused intersections, with reduced crossing distances to improve pedestrian connectivity along the street. Street corners will let you know you have arrived on 9th Avenue with intuitive wayfinding, incorporating unique and identifiable character elements of Inglewood like the river and the rails.

Street corners are a place to meet and will be configured to provide areas for community and public gathering. It is proposed that corners can also incorporate sustainable design elements that positively contribute to the environment as well as bicycle amenities to capitalize on proximity to bike routes and regional pathways. Major street corners can be community hubs and can provide public art opportunities throughout the street.

Design Strategies

SOUTH CORNER TREATMENT

- Railway/Industrial influenced seating area
- I-Beam and heavy timber gathering space
- Pedestrian character lighting
- Enhanced planting
- Artful pedestrian crosswalks + Integrated wayfinding
- Integrated bicycles parking and amenity



SOUTH CORNER AND GATHERING AREA TREATMENT

NORTH CORNER TREATMENT

- River influenced natural seating area
- Rain garden with enhanced planting
- Artful pedestrian crosswalks + integrated wayfinding
- Integrated bike parking and amenity
- Stormwater management

NORTH CORNER AND GATHERING AREA TREATMENT

Park Interfaces: Building on Community Assets

Parks along 9th Avenue provide an opportunity to build on current community assets. Parks will be a connected component of the street and provide the community with unique gathering areas. Landscape elements from parks can contribute and extend to the street, providing opportunities and areas for social interaction.

Design Strategies

- Wider sidewalks + new street trees to improve pedestrian realm and buffer park
- Extend park to the street
- Gathering opportunities at park edges
- Pedestrian lighting + seasonal lighting opportunities
- Seating and benches to buffer park from the street
- Permanent parking



MILLS PARK INTERFACE CONCEPT

Side Streets: Connecting People & Extending 9th Avenue

Side streets can provide a place that extends the community character to 9th Avenue. Bringing in elements of the surrounding area, like the river and the rails can make side streets identifiable and unique. Streets will be pedestrian focused and promote a slow paced street environment. Side streets can provide visible and physical connections to the community and surrounding destinations. Parking opportunities can be provided along the street while providing the potential for patio areas.

Design Strategies

- Intuitive and strategic wayfinding
- Improved visual connections to 9th ave and the community
- Street tree planting to improve canopy cover
- Wider sidewalks with corner curb extensions
- Opportunity for gathering - sidewalk patios and seasonal pop-up patios
- Enhanced street character
- Traffic calming elements
- Short-term on-street parking to support local businesses

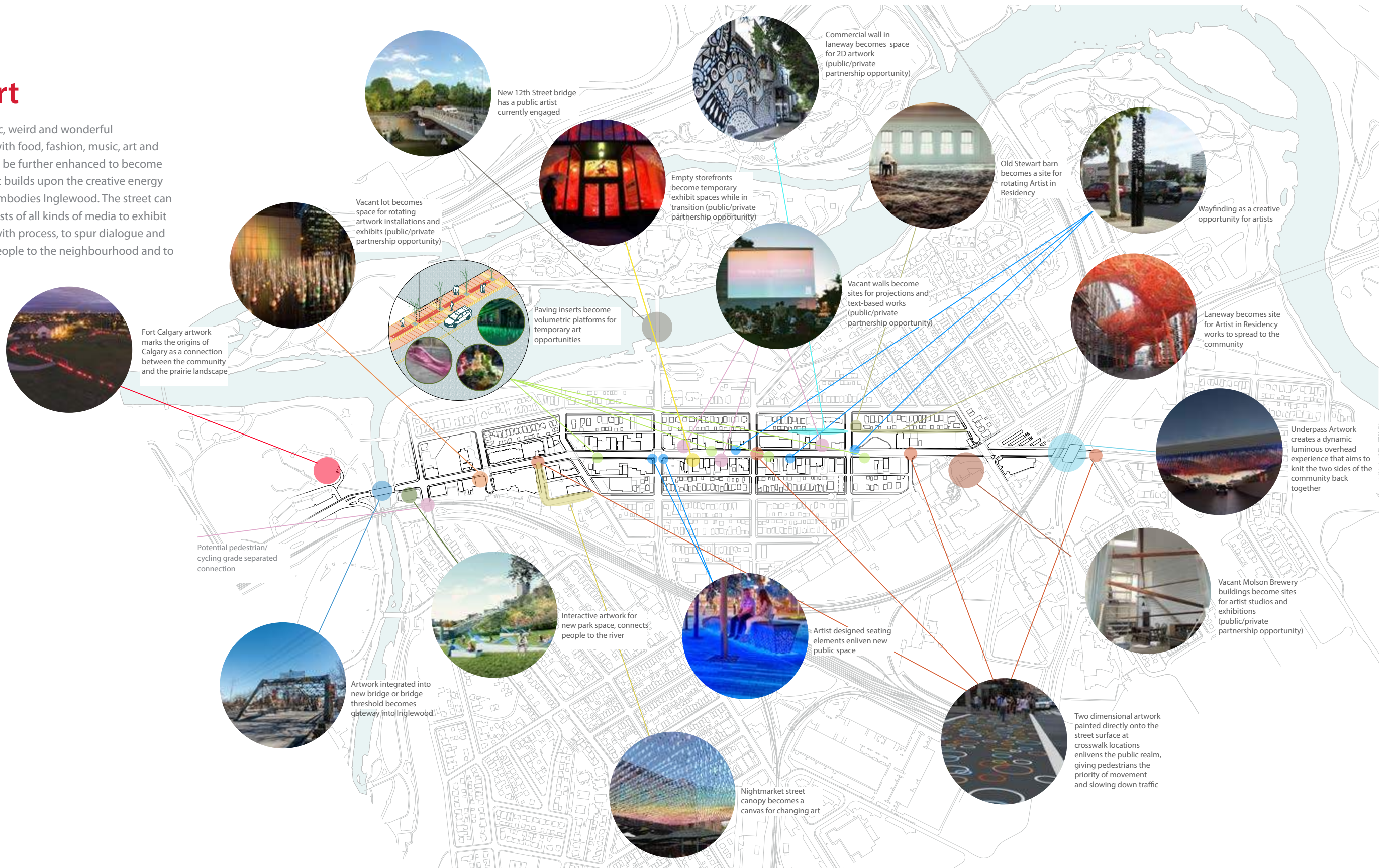


EXISTING

SIDE STREET CONCEPT - 11TH STREET

Public Art

Inglewood is an eclectic, weird and wonderful neighbourhood filled with food, fashion, music, art and design. 9th Avenue can be further enhanced to become a "cultural corridor" that builds upon the creative energy and momentum that embodies Inglewood. The street can become a space for artists of all kinds of media to exhibit work and experiment with process, to spur dialogue and debate, and connect people to the neighbourhood and to Calgary itself.



Pop-up Art Areas

Paving inserts become volumetric platforms for temporary art installations, providing artists with a street that is a canvas for creative expression. Installations can be flexible, seasonal, and local and reflect the weird eclectic character of the community.



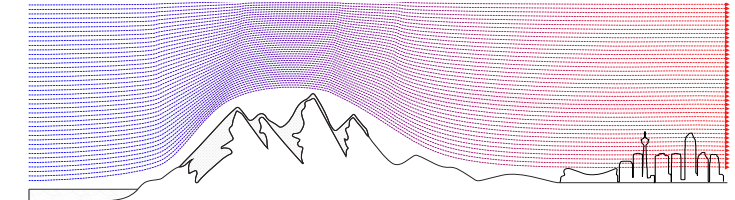
Gateways: Blackfoot Trail Underpass

The artistic intervention for the Blackfoot Trail Underpass is an opportunity to unite the two halves of the community back together while also creating an enticing transformative moment that connects together all Calgarians. The work is inspired by the unifying experience each and every living person in the southern Alberta prairie has experienced for thousands of years: the anticipation sudden of weather change seen across our giant prairie sky in the form of a Chinook Arch. The underpass also acts as a gateway for vehicles, pedestrians, cyclists, and transit users arriving and departing from the Blackfoot BRT stop.

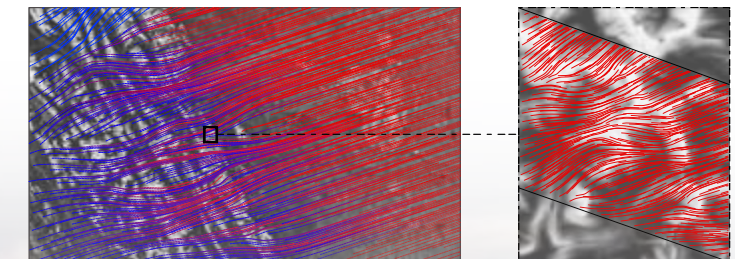
Design Strategies

The design strategies identified below are in alignment with the direction provided from one-on-one meetings with Transportation group:

- The design will include guard rails, new pedestrian paving and pedestrian lighting.
- The design preserves the existing minimum measured vertical clearance under the bridge of 4.66m at any point under the bridge.
- The design of the public art design that preserves sight distances along 9th Ave.
- Anchorage to the bridge soffit that takes into account the presence of high tensile steel strand in the bottom of the bridge girders.
- Inspection access and visibility of the bridge soffit will not be significantly reduced.
- Roads will not be responsible for the ongoing maintenance and repair, or removal of the public art at the end of its life.
- Cycling lanes will be clearly delineated.



Chinooks are formed when moist Pacific air rises over the Rocky Mountains and is condensed falling out as precipitation. The remaining dried air warms as it descends the leeward slopes leaving behind a band of stationary stratus clouds.

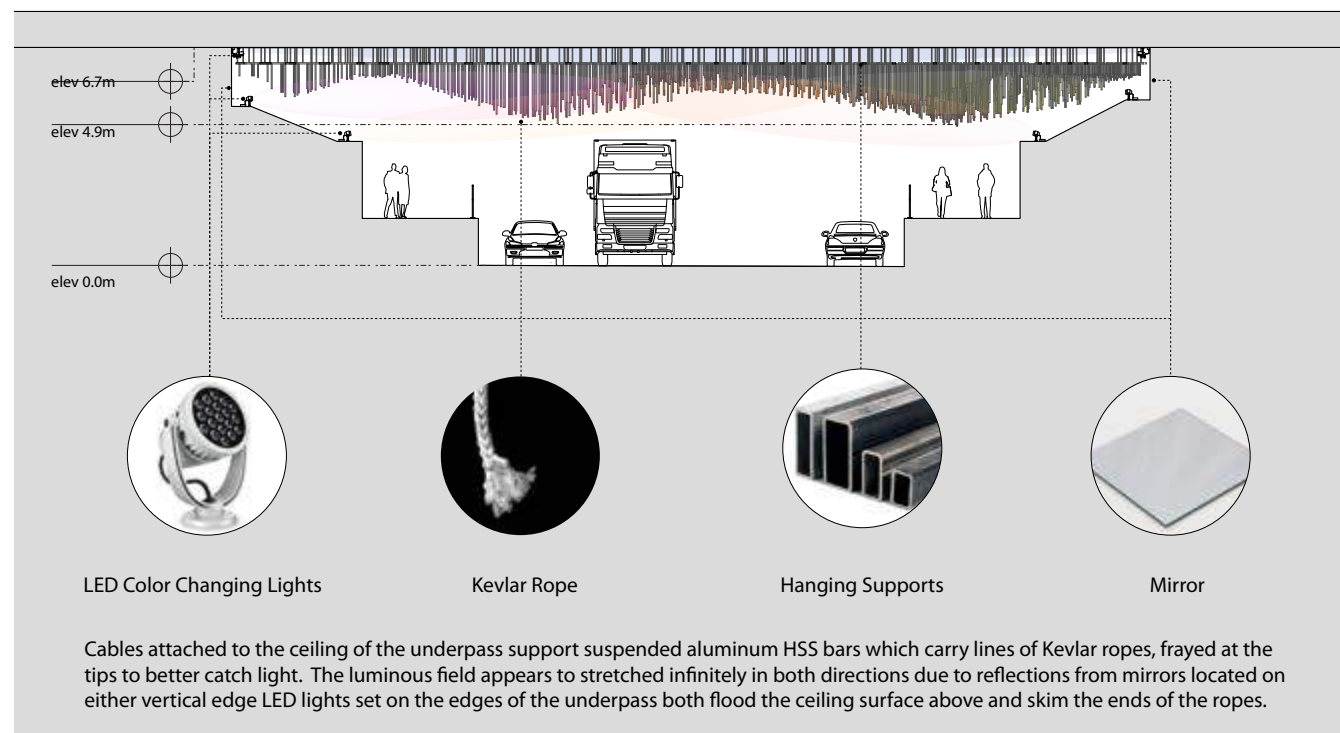


Turbulence of Pacific air moving west is three dimensionally mapped to create the topography and form of the suspended artwork.



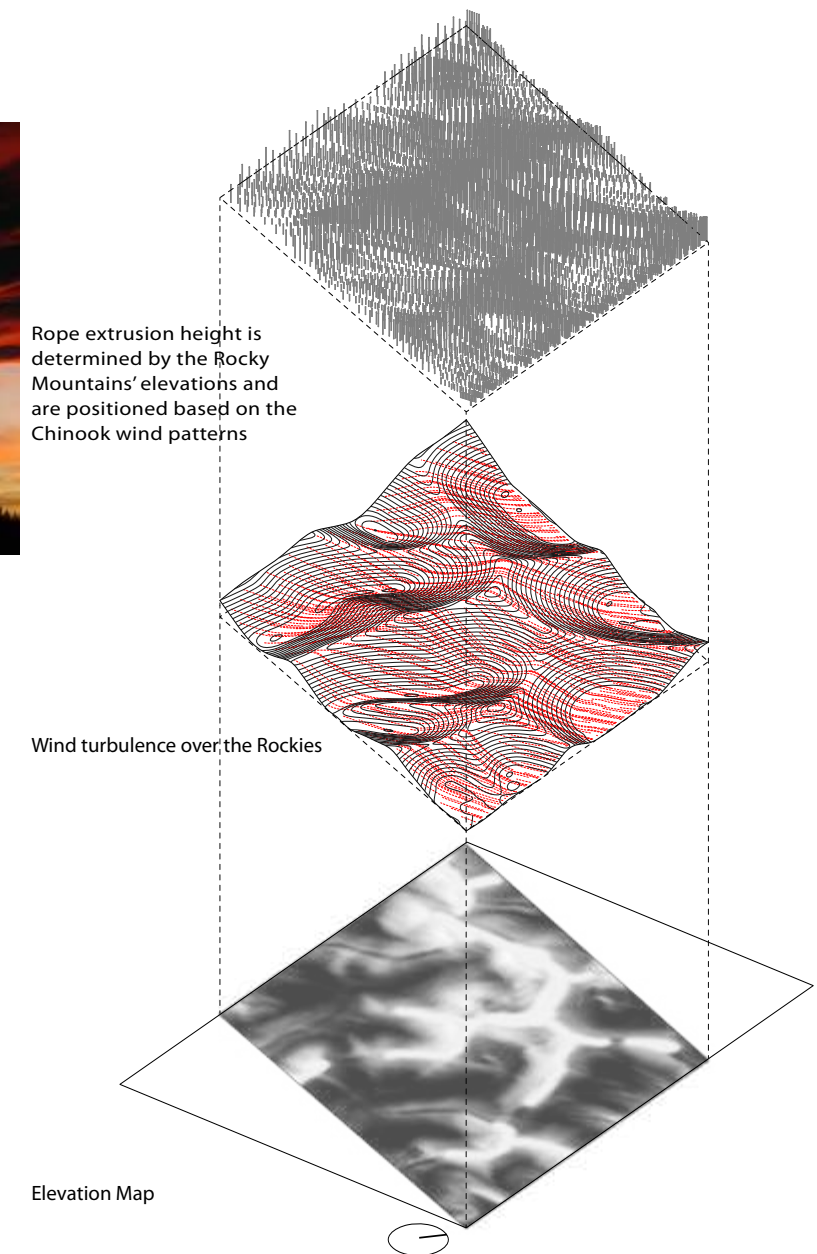


Concept Section



Red Sky at Night

Red Sky at Night creates a dynamic luminous overhead experience that highlights Inglewood’s changing nature and the phenomena of Albertan weather patterns. The work transforms a dark and uninviting pedestrian corridor into a dynamic and desirable space that is enlivened with a dynamic three dimensional ceiling installation that changes colour based real-time weather conditions and sways gently back and forth from both natural and traffic generated air currents.



4. Implementation

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Implementation

Outstanding Issues and Opportunities

The streetscape master plan sets the foundation for the development of the concept plan into the detailed design phase. The concept plan will be developed in more detail and additional analysis provided throughout this process. Specific elements and design areas will be explored in further detail in this phase.

Design Elements to be Explored Further in Detailed Design

- 12th Street laning and cross section.
- 9th Avenue laning and cross section between 16th Street and 20th Street.
- Intersection between 12th Street and 8th Avenue.
- Intersection between 9th Avenue and 17th Avenue.

Coordination with Parallel Projects

Various parallel projects are associated with the Main Streets Master Plan, and require or would benefit from close coordination between projects. This may result in schedule and cost-savings, reduced impacts on local businesses and residents, and/or long-term benefits for the community as a whole.

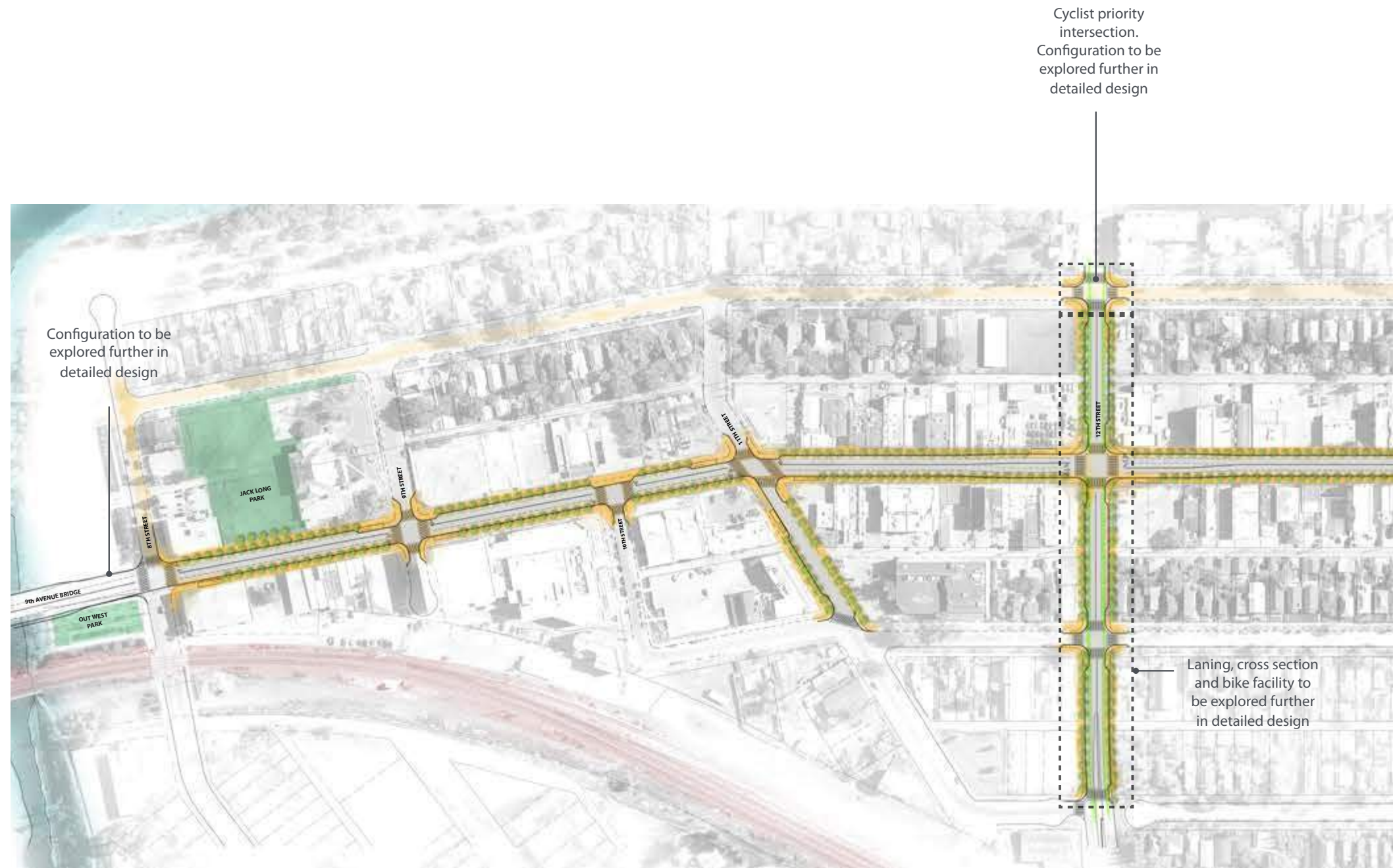
Phasing Approach

The phasing strategy considers timing and works of other capital projects to coordinate streetscape improvements. Additional consideration will be given to utilities upgrades along 9th Avenue. With active development permits and the build-out of various lots along 9th Avenue timing for individual blocks will be done in consideration of adjacent development.

In detailed design specific areas will be identified for potential pilot projects to sequentially implement and provide analysis of the streetscape design and street operations. Additionally phasing strategies will be explored further to provide alignment with other capital projects.

Next Steps

Currently, construction of the Master Plan is unfunded, but a capital funding request was submitted as part of the upcoming City of Calgary 2019-2022 budget cycle. Further capital funding updates will be provided in early 2019.

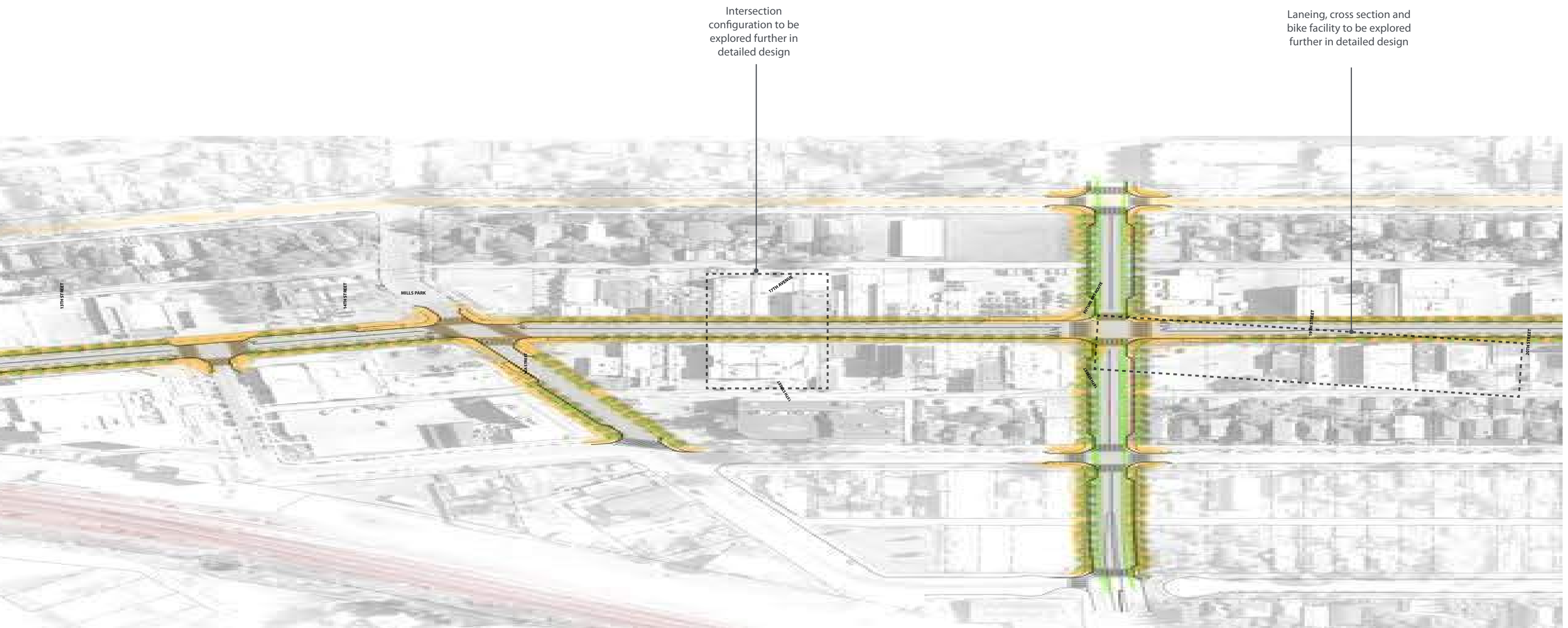


PHASE TWO

PHASE THREE

Phase Two

This phase is linked to the development and completion of the 9th Avenue bridge. With the temporary bridge and access road implementation of the streetscape design can align with this project. Specific areas linked to current developments such as the Atlantic and the Avili between 9th Street and 10th Street could be prioritize to link the streetscape improvements with adjacent development.



PHASE ONE

Phase Three

This phase is linked to the development of the Green Line and 12th Street station works. 12th Street can be maintained as an alternate connection with the sequential works associated with the 17th Avenue BRT and the 9th Avenue bridge replacement in previous phases of the streetscape work. Once access is reestablished in these areas work on 12th Street and the mid section of 9th Avenue can be completed.

Phase One

This phase is linked to the works with the 17th Avenue BRT connection and the development of the YWCA site. Work in this area can align with the completion of the BRT route and stations.

